



# STREETSCAPE PLAN & DESIGN TEMPLATES

FEBRUARY 2023

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*“Streets and their sidewalks, the main public spaces of a city, are its most vital organs.”*  
Jane Jacobs



# 01. INTRODUCTION & VISION

The Town of Springdale is surrounded by exceptional beauty. It is home to a vibrant community that shares a border with Zion National Park, offering magnificent views towards some of the world's most stunning canyons. Springdale also offers a rich cultural heritage, an eclectic collection of art galleries, world-class dining opportunities, and year-round recreational activities. Given these incredible amenities, Springdale has experienced significant growth over the last several decades. ***The intent of this plan is to guide the development of a safe, comfortable, attractive, and maintainable pedestrian streetscape environment within downtown Springdale while preserving the charming look and feel of the community for residents and visitors alike.***

For more than a decade, the Town of Springdale has made many improvements to vehicular and active transportation within the community to improve the overall user experience. Previously completed plans include the 2008 Zion Corridor Multi-Use Trail Feasibility Study, the 2017 Springdale Parking Management Study, the 2018 Springdale Wayfinding and Signage Study, the 2020 Zion Corridor Multi-Use Feasibility Follow-up Study, the 2021 Springdale General Plan, and the recently completed 2022 Springdale Downtown Transportation Study and Active Transportation Plan. Many of these completed plans have resulted in significant improvements to the community, such as the construction of a bicycle and pedestrian trail to connect Rockville, Utah to Springdale, the implementation of a paid parking system, the addition of continuous bike lanes and improved sidewalks, and the addition of pedestrian wayfinding signage. Many of these studies, including the 2021 Springdale General Plan, have recommended that the Town develop and enhance a pedestrian-oriented streetscape with increased connectivity to and through open space. This Streetscape Plan and Design Templates document will recommend improvements to enhance the appearance of the streetscape and will provide guidance on ways to increase pedestrian comfort and safety through design templates and strategies. This document will also provide a concept plan for a passive community recreation space within a Town-owned ('SR-9 Property') parcel in the heart of the downtown area.

The following goals for the Streetscape Plan and Design Templates document were derived from Stakeholder and Town staff directives and feedback from residents, business-owners, and the greater community. To be successful, the plan should:

- **Improve the appearance of SR-9**
- **Create a safe and consistent pedestrian environment**
- **Complement the surrounding scenery while encouraging pedestrians to walk through Town**
- **Create a new small public gathering space in downtown**

- **Create a more flexible and ‘usable’ streetscape that allows for more opportunities for seating, shade, gathering, festivals, and outdoor dining**
- **Provide a road map that can be used to guide decisions for enhancing Springdale’s streetscape**

This plan considers a wide range of options while being cautious about altering the existing streetscape’s character and function too much, as many of these existing elements are vital to Springdale’s unique charm. This plan establishes an overall look and feel for the streetscape that can also be used to evaluate private requests to make changes to the streetscape as the community continues to grow and prosper.

## THE PLANNING PROCESS OVERVIEW

A collaborative public participation and stakeholder outreach process was used to develop the Streetscape Plan and Design Templates document. Responses from stakeholders and residents were used to evaluate the applicability of potential enhancements to the streetscape through a Visual Preference Survey. The planning process was completed over a six-month period and provided multiple opportunities for residents, property owners, merchants, stakeholders, and Town staff to contribute to the plan. This included:

- Small stakeholder group interviews with business owners, property owners, and residents.
- A public outreach event that featured a walking tour of the existing streetscape environment.
- In-person and digital Visual Preference Surveys to collect opinions and ideas on what the community would like to see in downtown Springdale.
- Several design concept review meetings with stakeholders and Town staff to review the various ideas developed for the streetscape and for the SR-9 Property.

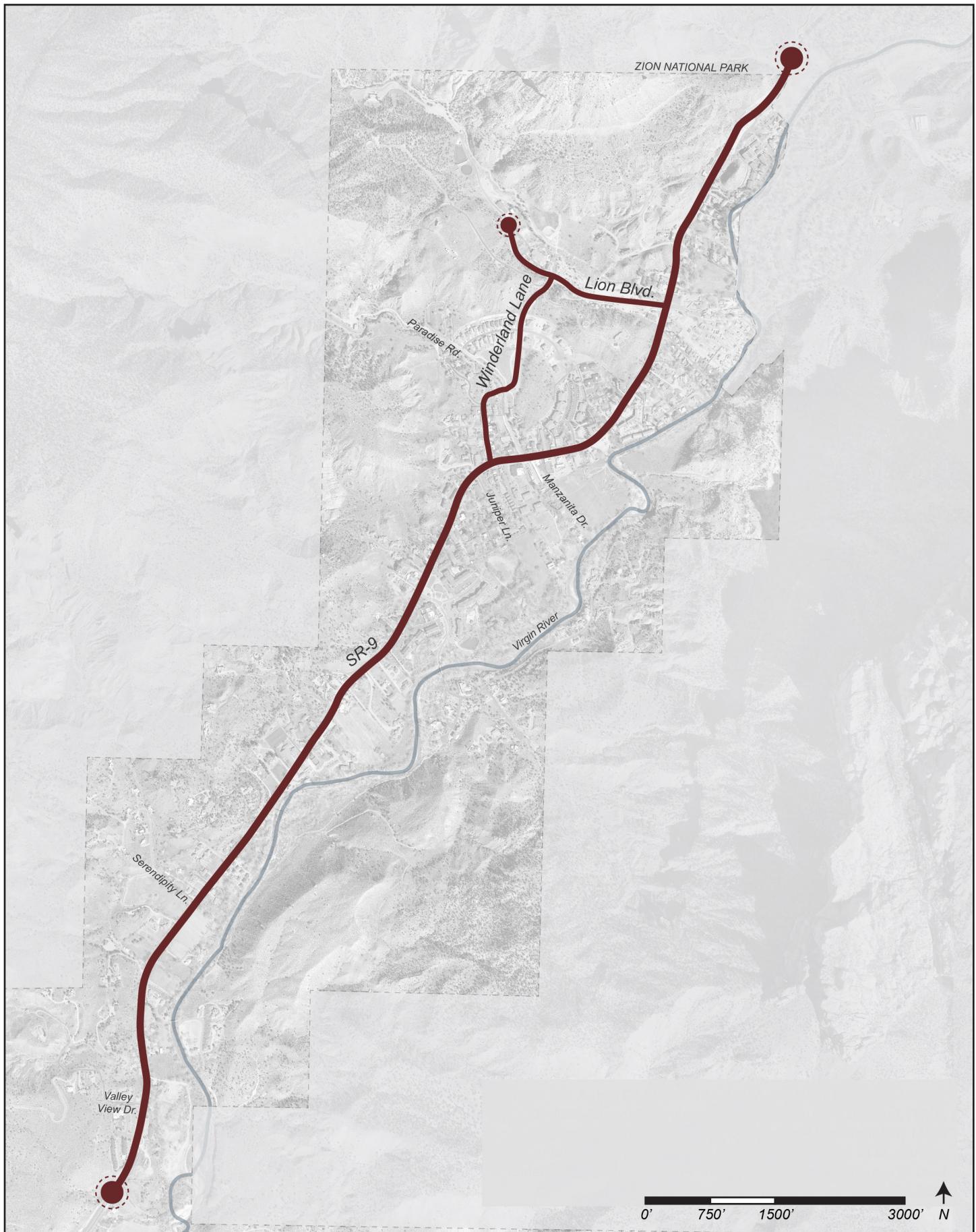
Throughout the process, Town staff were available to take comments and suggestions via phone and email.

## THE STUDY AREA

The study area for the Streetscape Plan and Design Templates document incorporates the heart of downtown Springdale along SR-9 and a couple of distinct side streets within the Town center as shown in the map to the right. These side streets include Lion Boulevard, which leads to the Town office complex and community center, and Winderland Lane. The southern limit of the study area along SR-9 begins at the Majestic View Lodge while the northern limit is at the southern entrance to Zion National Park.



*Stakeholder Discussion at the SR-9 Property*



Study Area Overview



*"It takes a place to create a community, and a community to create a place."*

*Fred Kent*



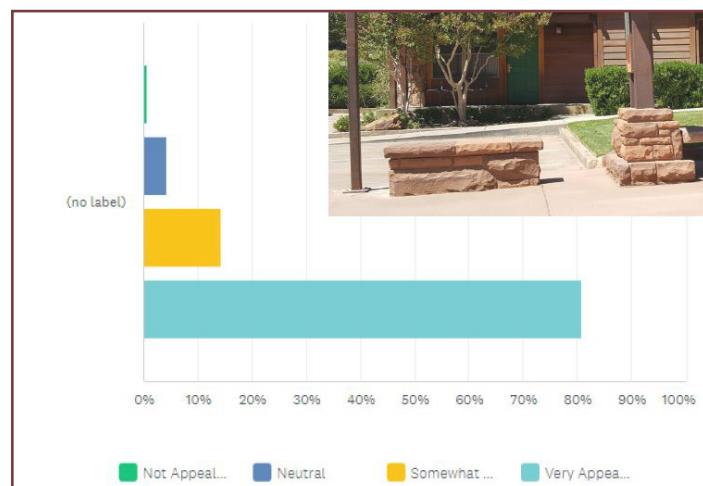
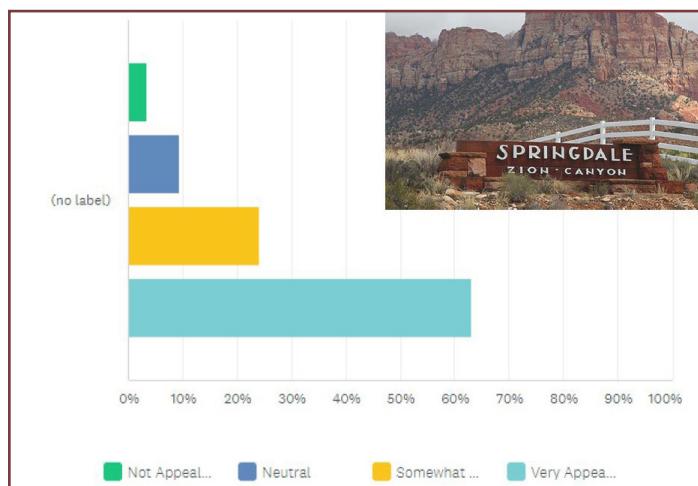
## 02. EXISTING CONDITIONS

As recently noted in the Springdale Downtown Transportation Study and Active Transportation Plan, Springdale's most notable constraint is the quarter mile wide valley in which the Town is located. All development and roadway infrastructure is funneled along SR-9 which leads directly into Zion National Park. While the Town of Springdale has a population of just over 550 residents, the National Park received over 5 million visitors in 2021. It is important to recognize that **all** vehicular, bicycle, and pedestrian travel into and out of the southern entrance to Zion National Park must occur along SR-9 through Springdale.

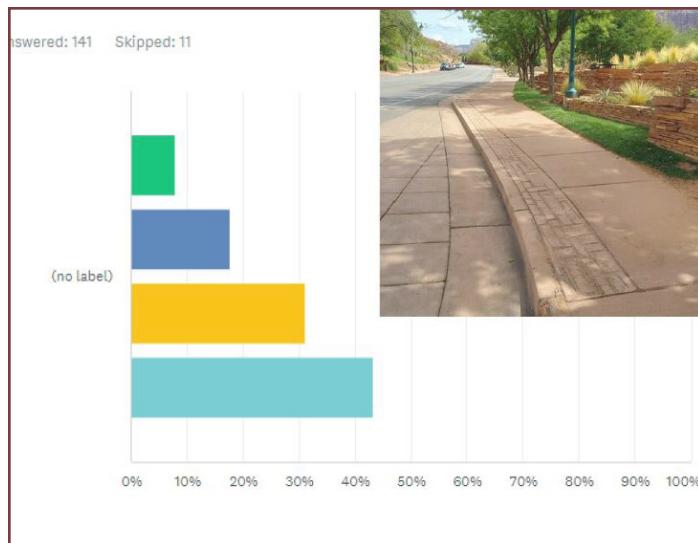
SR-9 is a two-lane roadway with varied right-of-way widths. In the southernmost segments, the available right-of-way spans nearly 200' while in the downtown area the right-of-way is limited to just under 65'. The right-of-way grows to closer to 100' near the entrance to the National Park. As noted in the Springdale Downtown Transportation Study and Active Transportation Plan, steep terrain and the adjacent Virgin River to the east of Springdale largely prohibit the Town's ability to build a secondary route to access the National Park. As such, the streetscape environment along SR-9 is critical in creating a safe, comfortable, and inviting setting for more than 5 million people annually as they access one of the nation's greatest treasures.

### ASSETS AND OPPORTUNITIES

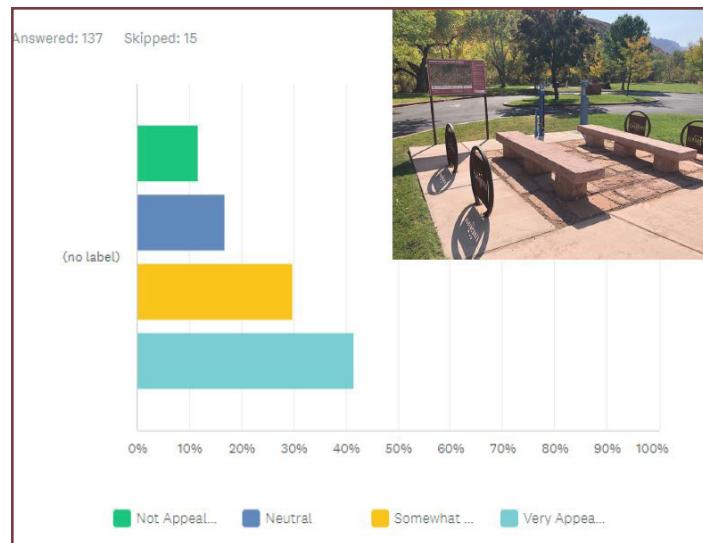
The Town of Springdale is a beautiful destination of its own, with the shared border to the National Park as an added bonus. The results of the Visual Preference Survey show that the community loves many of the existing features that are found within the Town and is a testament to the strong visual quality that already exists in the area. As noted in the graphs below, the existing stone seating that occurs within the shuttle stops along SR-9, Springdale-branded bicycle racks, Springdale's entry monument to the south of Town, and decorative pavement patterns are all highly desirable to the community.



## EXISTING CONDITIONS



*Approval of Existing Paving Patterns*



*Approval of Springdale-branded Bicycle Racks*

The Town has succeeded in capturing the unique character of the area through natural textures, materials, and color tones within these elements and other existing features such as retaining walls, accent stone walls, and the Springdale shuttle stop shelters. The shuttle stops offer amenities such as much-needed shade in the summer months, stone seating, water bottle filling stations, and stone-clad trash receptacles. Existing sidewalks and decorative street lights exist throughout many portions of the study area and are nicely maintained. The sidewalks feature ample width to accommodate the volume of visitors that frequent the area while the existing street lights offer dark-sky compliant safety lighting.

The Town has also incorporated two decorative paving treatments to highlight the historic canal and ditch systems that once lined the street. These elements are favorably viewed by the community in terms of their aesthetics, but many residents are not aware of their specific purpose. An existing plaque describing the historic canal and ditch system exists that could be better tied to the paving treatments. The decorative paving treatments will be highlighted as part of this streetscape plan through interpretive plaques or panels in the pavement to better identify their history and purpose.



*Existing Shuttle Stop & Amenities*



*Existing Accent Walls*



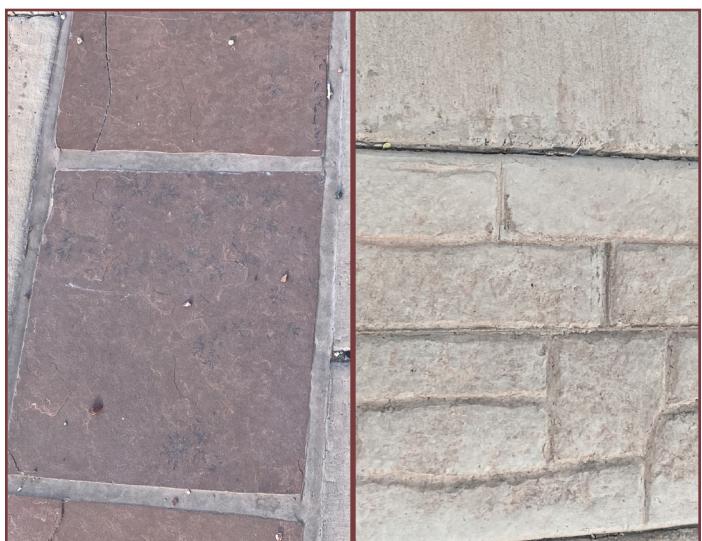
Many of the businesses adjacent to the SR-9 corridor feature inviting storefronts with local art, colorful plantings, or outdoor dining opportunities. These semi-public spaces abut the existing sidewalk and effectively expand the streetscape environment beyond the fairly limited public right-of-way. There is tremendous opportunity within these areas to partner with local businesses to incorporate more seating, passive recreation areas, shade elements, interactive features, and local art. These amenities are highly desired by the community as evidenced by the results of the Visual Preference Survey and will significantly enhance the overall streetscape environment.

In addition, the artist community of Springdale has contributed substantially to the aesthetic quality of the area through the colorful mosaic pieces along Lion Boulevard and playful metal sculptures such as the Springdale Tortoises. There are five Springdale Tortoises located throughout the community that have the opportunity to be further highlighted as part of the streetscape plan.

The intent of this plan is to build upon these existing, successful streetscape elements and to encourage continued use of many of these same or complementary features in areas that are currently lacking amenities.



*Existing Decorative Street Lights*



*Existing Decorative Pavement Treatments*



*Existing Wind Sculptures at Worthington Gallery*



*Springdale Tortoise at Existing Dog Park*

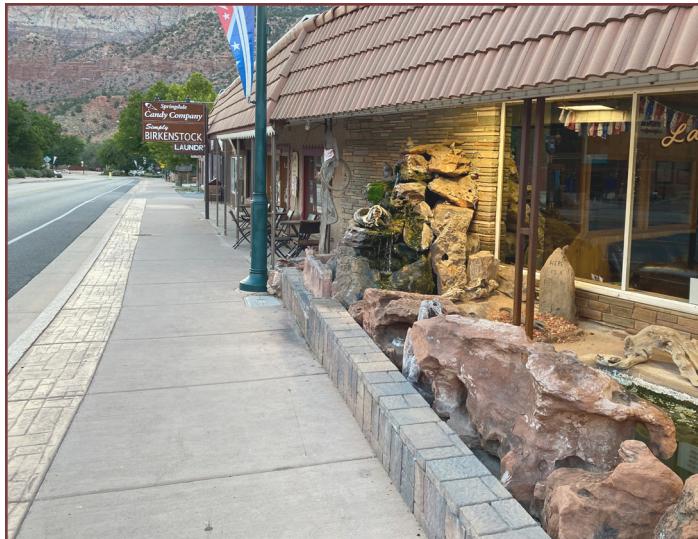
### CHALLENGES AND CONSTRAINTS

Despite the current overall success of the SR-9 corridor, a number of challenges and constraints were identified by the project team, stakeholders, and residents during the planning process. As previously mentioned, one of the most notable constraints is the limited right-of-way that is available for streetscape improvements within much of the downtown area. The limited right-of-way makes partnership critical with adjacent property owners and businesses to allow for increased amenity opportunities or to provide enough space for additional plantings or shade trees.

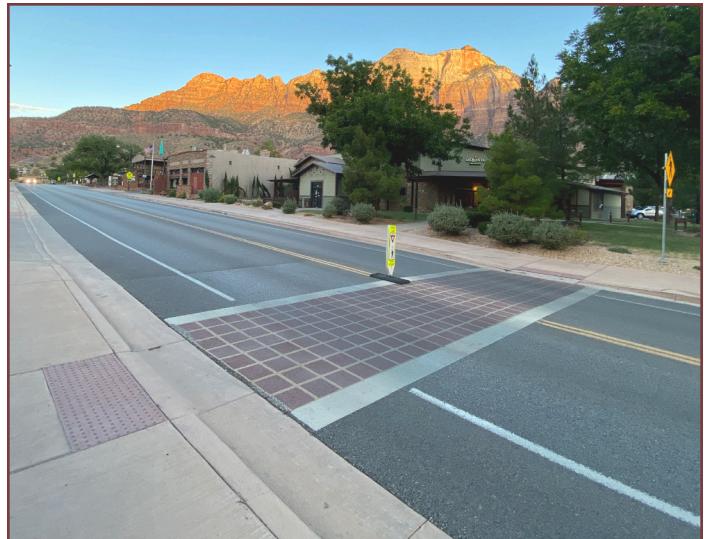
Several significant gaps in sidewalk connectivity exist along the west side of portions of SR-9. These gaps cause pedestrians to potentially walk in the roadway or cross at unmarked locations to access existing sidewalk or pathway on the east side of the street. There are currently eight marked crosswalks within the study area due to the volume of pedestrian traffic. While standard pedestrian warning and crosswalk signage exist prior to each crossing, the signage is not consistent. In addition, the existing crosswalks are not designed to accommodate bicyclists. Several residents and stakeholders commented on the need for more crosswalks. Specifically, a new crosswalk just south of the Lion Boulevard intersection and another new crosswalk near the entrance to the National Park were mentioned due to the high volume of visitors crossing in these areas. They also commented on the need to enhance the existing crosswalks to make them more visible, although many were concerned about adding additional lights or flashing beacons to these locations.

Another notable challenge area is located near the pedestrian entrance to the National Park. As previously mentioned in the Springdale Wayfinding and Signage Study, there is not a clearly defined route that guides pedestrians and bicyclists from SR-9 towards the entrance. The right-turn lane on SR-9 and the bike lane access into this area also create conflicts with one another. Zion Canyon Village has made several improvements over the last few years such as adding more signs and creating dedicated bike lanes along their entry drives; however, many people still continue to try to enter the National Park off of SR-9. A visual pathway guiding visitors along SR-9 down into Zion Canyon Village and to the entrance to the National Park is critical to improving the navigation of the area.

Bicyclists and E-bike riders were noticed riding on the sidewalks in some instances during field observations. A concerted effort is being made by the bike rental community to discourage this



*Limited Public Right-of-way in Downtown*



*Existing Crosswalk*



behavior, but the risk of conflict between different modes of travel still exists within the streetscape environment. The numerous driveways along the corridor also create potential conflicts between vehicles and streetscape users.

There are several areas along the corridor that feature unshaded seating areas that don't reflect the overall look and feel that the community desires, such as several unshaded wooden benches along the corridor. Shade was a significant concern to the large majority of residents and visitors. Temperatures can soar in the summer months which coincides with much of the peak visitation to the National Park. It was also noted that some of the shuttle stops are not shaded. In addition, some stretches of sidewalk feature very little shade. One section of sidewalk is also located next to an existing segment of the historic canal, which unfortunately creates a safety concern due to the grade difference between the canal and adjacent sidewalk.

Other challenges noted within the area include the need for more public restrooms, pet amenities such as pet drinking fountains, traditional water fountains rather than bottle filling stations, deer grazing along the roadway, and more seating opportunities.



*Unsafe Pedestrian Crossing*



*Bike Riders Nearing Zion Canyon Village*



*Existing Wood Bench with Limited Shade*



*Existing Segment of Historic Canal*



*“The future of every community lies in capturing the passion, imagination, and resources of its people.”*

Ernesto Siroli



## 03. PUBLIC OUTREACH

The key insights, concerns, and ideas contributed by residents and stakeholders who were involved in the planning process are summarized in this section. The primary outreach efforts included bi-weekly meetings with the Springdale Streetscape Committee, an online Visual Preference Survey that was offered to the community, small group meetings with key stakeholder groups, and a community meeting that included a walking tour of the existing Springdale streetscape. While each and every comment cannot be included in this section, the common preferences will be highlighted. A more detailed summary of the comments is provided in the Appendices.

The Team's approach to developing a community-inspired streetscape plan was to employ strategies and initiatives to engage, communicate, and gain community insight for recommended aesthetic appeal, features, and amenities. Reaching a diverse set of community member and stakeholder groups was critical to ensure many voices were heard. The Team worked with the Springdale Streetscape Committee to develop a comprehensive Visual Preference Survey and to identify the appropriate stakeholder groups to connect with. In addition, outreach to the general public was equally important to gain insights on what area residents want to see in their community. Given the complex dynamic between a tremendous amount of yearly visitors and the relatively small group of residents, it was critical to plan not just for the masses but also for those who call Springdale home.

### VISUAL PREFERENCE SURVEY

An online Visual Preference Survey was developed to gather community input on the desired aesthetic appearance of the streetscape. The survey was advertised to the community and left open for over a month. The goal was to understand what participants liked or didn't like about sample images that depicted a range of streetscape amenities or public gathering spaces. Included in the survey were images of the following:

- Signature Entry Feature
- Enhanced Sidewalks
- Enhanced Crosswalks
- Interactive Design Elements
- Self-guided Visual Pathway
- Small Community Park Activities
- Public Gathering Nodes
- Public Art
- Shade Elements
- Site Furnishings
- Bike Racks
- Landscape Lighting

Over 160 responses were received on the survey. Of the 144 individuals who chose to provide their demographics, over 100 identified as Springdale residents. Refer to the chart on the following page for a detailed breakdown of the respondent demographics. The responses show that the community has a strong preference for the look and feel of many of the amenities that the streetscape along SR-9 already offers. Particularly clear was the appreciation of the existing monument sign on the

ANSWER CHOICES	RESPONSES	
I am a resident of Springdale	72.22%	104
I own a business in Springdale	26.39%	38
I work in Springdale	37.50%	54
I am a resident of a nearby community	18.06%	26
I own a business in a nearby community	4.86%	7
I work in a nearby community/location	3.47%	5
I am a visitor to Springdale	0.69%	1
Other (please specify)	Responses	9.03%
		13

**Total Respondents: 144**

## *Visual Preference Survey Demographic Results*

south side of Town along with a preference towards the natural textures and colors of the stone benches and paving textures. The existing Springdale-branded bicycle racks were also viewed favorably. The survey shows that there is a strong desire for more of these same amenities, and significant support for the possibility of creating more publicly accessible passive recreation and gathering spaces. More shade and low-level pedestrian lighting were also appealing to respondents. Additional support was shown for interpretive or educational opportunities through art installations or signage.

The survey makes it clear that the Town has already succeeded in defining an appropriate visual character for the community. Respondents had the strongest negative impressions of elements that began to deviate from this character. There was also a fairly negative impression of adding interactive features within the streetscape environment due to the potential of adding congestion to an already busy area. However, many of the stakeholder groups expressed interest in adding features such as viewing binoculars or interactive art. As such, any interactive elements must be placed so that they do not impede traffic flow or cause disruption to adjacent businesses or residences.

There was a strong preference to avoid adding additional overhead lighting to crosswalks; although in some circumstances maintaining public safety must be at the forefront. This plan uses the results of the Visual Preference Survey to guide the overall aesthetics of the Springdale streetscape environment while balancing safety, comfort, and maintenance needs. A summary of all responses is provided in the appendices.

## **STAKEHOLDER MEETINGS**

The Team held eight (8) small-group stakeholder meetings with individuals that represented a diverse range of interest groups. All but two of these meetings were held in-person. The other two meetings were held virtually via Microsoft Teams meetings. Stakeholders included Utah Department of Transportation, Zion National Park, Zion Canyon Visitor's Bureau, outdoor enthusiasts, art enthusiasts, healthy lifestyle advocates, bicycle enthusiasts, history buffs, business owners, and school representatives.

The purpose of these sessions was to identify specific concerns or interests that each of these groups has regarding Springdale's overall streetscape environment. The following is a list of key take-aways

from these discussions:

- The streetscape desperately needs shade.
- More seating is necessary, and not just at the shuttle stops.
- There needs to be more trash cans or more accountability from adjacent restaurants to empty their trash.
- Many people are in favor of public restrooms at the SR-9 property.
- Bicycles on the sidewalks is a big concern.
- Crosswalks need to be improved: location, material, and visibility.
- Pedestrian accessibility and identification need to be improved at the National Park's entrance.
- Implementation of public art should be purposeful and meaningful.
- Interpretative signage highlighting history, geology, culture, or astronomy would be beneficial throughout the Town.
- There needs to be more representation of the Paiute people and history in the area.
- Support for small water feature at SR-9 property with caveat of finding a solution to water usage.
- Springdale shouldn't become a 'typical gateway town' - it needs to maintain its charm and character.

A summary of the stakeholder meeting notes is included in the appendices.

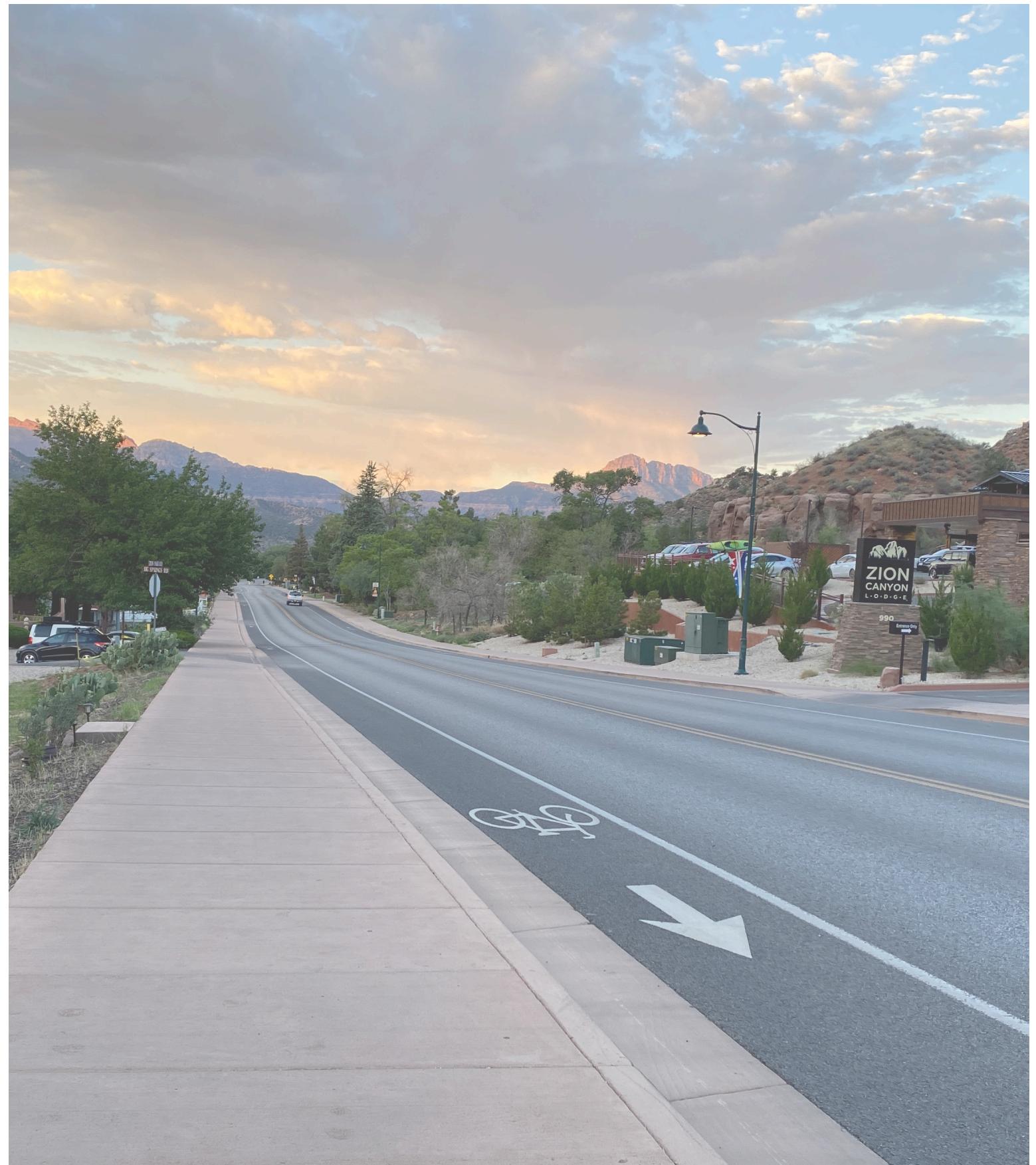
## PUBLIC OUTREACH MEETING

A public outreach meeting was held at the Bumbleberry Theatre on September 1, 2022. The meeting began with a brief introduction of the project followed by a walking tour of several sections of the existing streetscape. Following the walking tour, community members had the opportunity to weigh in on their preference for various amenities via display boards. Many of the same challenges identified during the small group stakeholder meetings were heard. A summary of the input received during the public outreach meeting is included in the appendices.

Elementary school students were also given an opportunity to provide input on the streetscape environment. They were given colored dots and were asked to 'vote' on images that they saw as desirable. The students preferred many of the same natural features that were liked by the broader community. However, unsurprisingly they also gravitated towards more of the fun, interactive features that were met with less support from the overall community. These results show how important it is to collect feedback from as many different voices as possible to provide a truly community-inspired streetscape environment - one that is appreciated by not just the current generation, but the future generations as well.



Streetscape Walk with Community Members



*"The need for connection and community is primal, as fundamental as the need for air, water, and food."*

Dean Ornish



## 04. STREETSCAPE PLAN

Good streetscape design should be functional, maintainable, and attractive. It should build upon the existing character of the community while creating a unique sense of place that draws visitors into a comfortable, safe, and interesting environment where people want to linger. These are the values that have come together to create the concepts developed as a part of this Streetscape Plan & Design Templates project. The concepts that follow explore a wide range of options that work within the existing streetscape framework to reflect the needs and interests of the community. Many of the options are easily implementable in the short-term, while others will require further design and coordination with Town staff and the community prior to implementation.

Given the varied right-of-way and existing streetscape conditions along SR-9, the project area has been divided into six (6) typical segments as described on page 16 and as shown on the map on page 17. These segments provide an opportunity to customize the streetscape recommendations so that they work within the diverse context of the SR-9 corridor while still maintaining an overall look and feel for the community. Many elements will be repeated within each segment to create consistency and maintain a sense of place. These elements include some of the following:

- Springdale-branded bicycle racks
- Pedestrian-scale bollard lighting in activity nodes
- Artwork enhanced electric boxes
- Driveway access paving treatments along SR-9
- Accent plantings
- Self-guided visual pathway along SR-9
- Decorative planters
- Interpretive signage

These elements, along with all other recommended improvements, are described in further detail in the following pages and within Section 5: Design Templates.



*Artwork Enhanced Electric Box*



*Bollard Lighting*

## OVERVIEW

The Segment Overview map to the right provides the location of the six (6) typical streetscape design segments that have been identified for the Town of Springdale based on their unique characteristics. The typical streetscape design segments are as follows:

### RURAL SEGMENT

The Rural Segment of SR-9 extends from the Majestic View Lodge to just northeast of Apple Lane and offers visitors a first look at Springdale as one approaches from the South. It features a paved, multi-use pathway on the east side of the road along with a mixture of farmland, residential areas, lodging, and businesses. The pathway is currently protected by a post and cable barrier in many locations along the corridor.

### TRANSITIONAL SEGMENT

The first Transitional Segment of SR-9 extends from just northeast of Apple Lane to south of the Sol Foods Supermarket. The second Transitional Segment extends from Desert Pearl Inn north to Feel Love Coffee. These segments see higher volumes of foot traffic and feature greater densities of lodging, shops, and dining opportunities. The multi-use pathway from the Rural Segment is transformed into a wide sidewalk that extends all the way to the pedestrian entrance of Zion National Park.

### DOWNTOWN SEGMENT

The Downtown Segment of SR-9 takes visitors through the heart of Springdale from the Sol Foods Supermarket to Desert Pearl Inn. Lined with lodging, shops, dining, and art galleries, this segment features high volumes of foot traffic. Many of the neighboring businesses offer outdoor seating, eclectic artwork, colorful landscaping, and casual seating opportunities. Existing decorative pavement interpreting Springdale's historic ditch system lines both sides of the street.

### NORTHERN SEGMENT

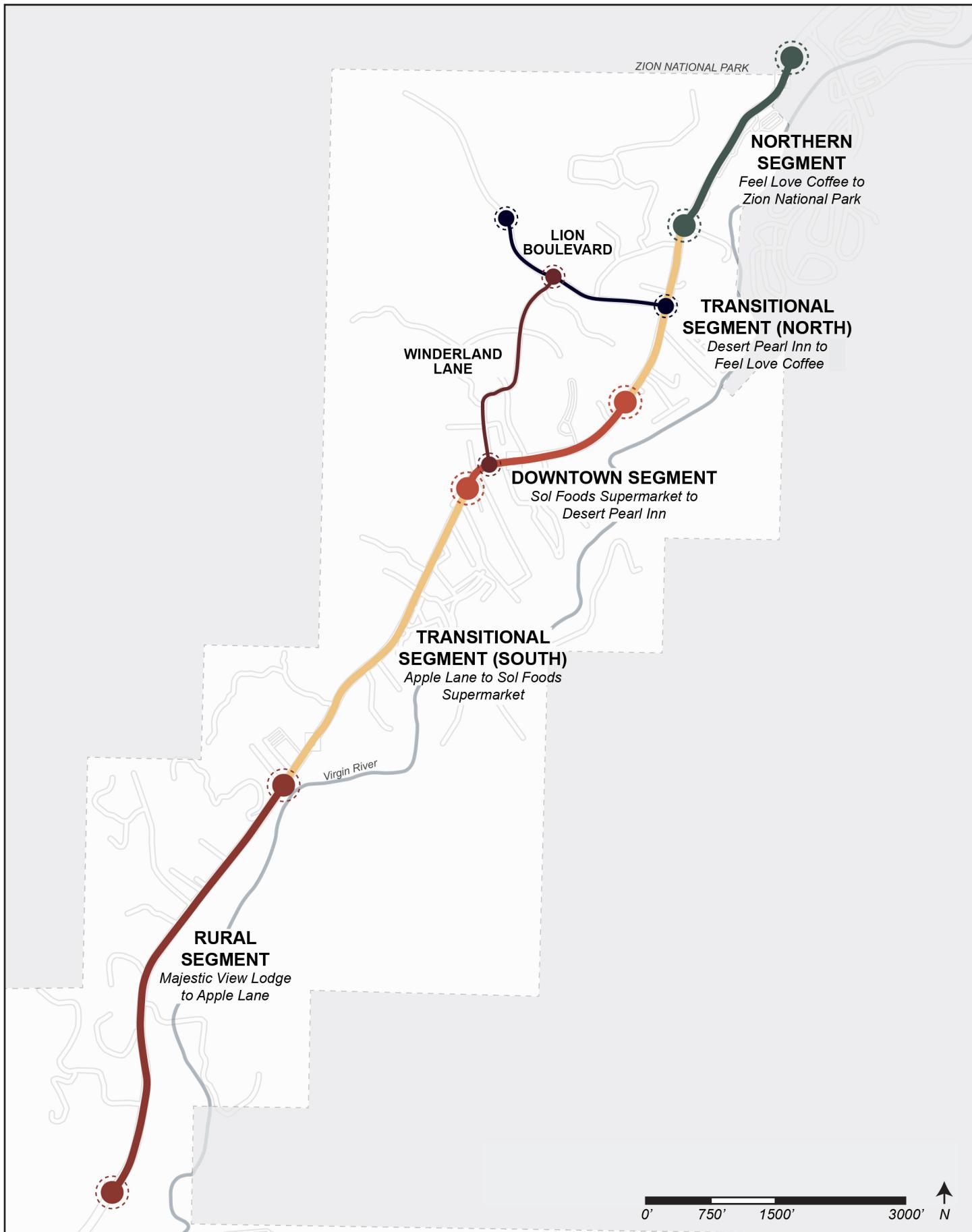
The SR-9 Northern Segment extends from Feel Love Coffee to the Zion National Park entrance gates. For visitors entering Springdale from the Park, this segment offers a first look at the community. This segment features dramatic views of the surrounding peaks, existing retaining walls, and a continuous sidewalk on the southeast side of SR-9. Lodging, shopping, and dining opportunities are featured in this segment as well as the pedestrian entrance to the Park.

### LION BOULEVARD

Lion Boulevard provides visitors a glance at what life is like as a Springdale resident. The community center, library, Town park, and government offices are all accessed along this route. Lion Boulevard also features Lion Boulevard Mosaics, a community-driven art project that depicts canyon-inspired themes through tile mosaics on lamp posts, wall panels and utility markers. Visitors also enjoy convenient, lower cost parking along most of the street. Shade trees have been planted along portions of the street and cast much-needed shade in the summer months.

### WINDERLAND LANE

Winderland Lane transitions from an active commercial area near its intersection with SR-9 to a quiet residential street near Paradise Road, and then back to a more active civic area at the intersection with Lion Boulevard.



Segment Overview Map

## RURAL SEGMENT

Several improvements are recommended to enhance the streetscape character of the SR-9 Rural Segment, including adding shaded seating areas with stone benches that match the look and feel of the existing shuttle stop shelters, replacing the existing post and cable barrier with a decorative and longer-lasting 6" concrete curb, incorporating accent plantings and shade trees along the pathway where feasible, continuing the use of the existing accent walls in key areas to promote streetscape consistency, and including interpretive signage and interactive elements in seating nodes. This segment will also introduce the concept of the Self-Guided Visual Pathway to Zion National Park through the use of curved, concrete curbing adjacent to the multi-use path.

The following is a list of recommended improvements. Refer also to the map on page 19.

## SAFETY IMPROVEMENTS

- A** Upgrade existing signalized crosswalk at Majestic View Lodge to Rectangular Rapid-Flashing Beacon (RRFB) with crossing nodes. Refer to Section 5 for Crossing Node design template.
- B** Install new stamped concrete crosswalk at Serendipity Lane with in-ground lights and node crossings. Refer to Crossing Node design template.
- Provide pedestrian-scale bollard lighting at activity nodes where feasible.

## PUBLIC/PRIVATE PARTNERSHIPS

- C** Consider partnering with the existing entry monument property owner to enhance monument with landscape improvements and additional rock wall features.
- D** Consider partnering with **Canyon Vista Lodge** to further enhance recommended activity node in area.

## ACTIVITY NODES

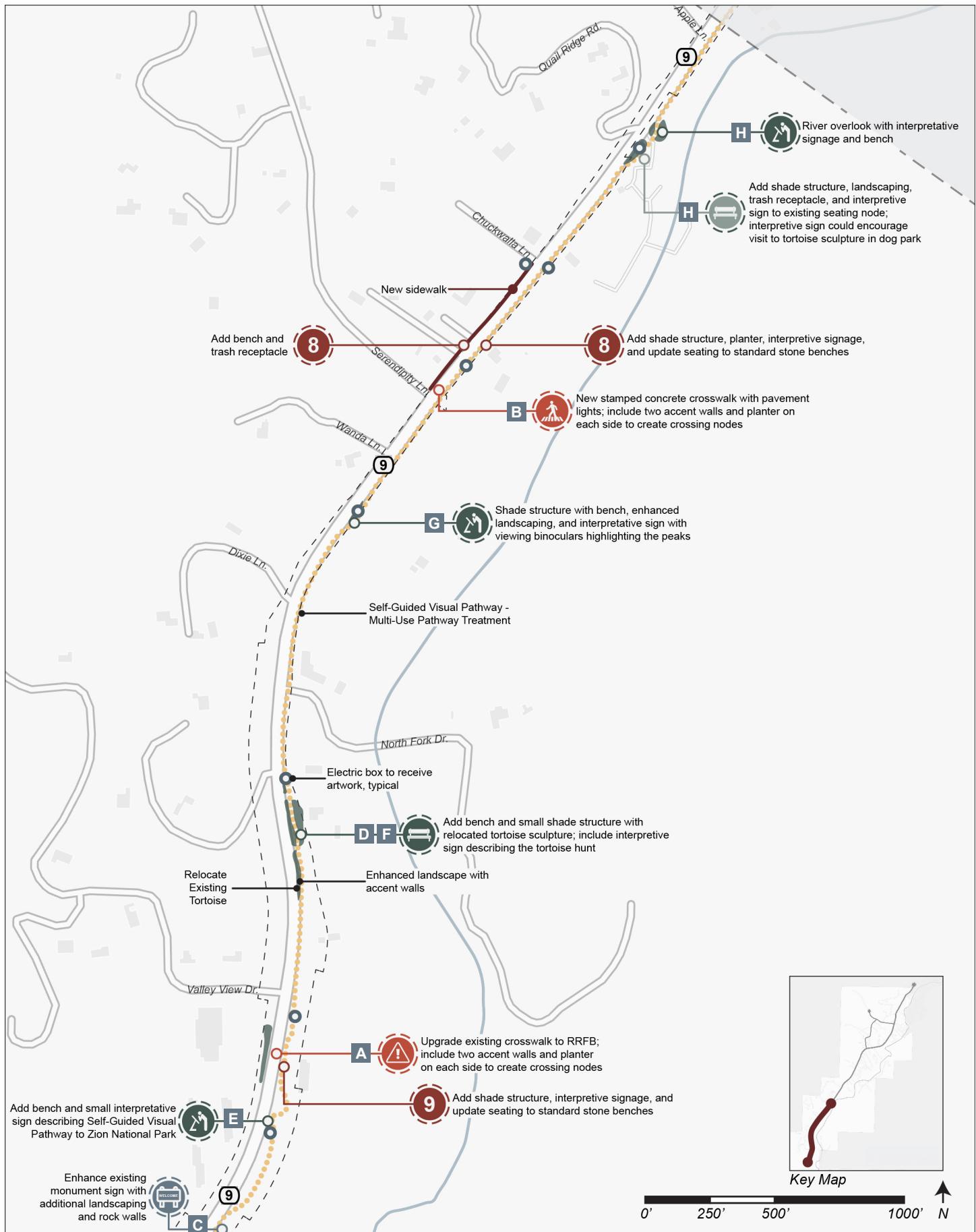
- E** Create seating area with small interpretive sign describing Self-Guided Visual Pathway to Zion National Park near the Majestic View Lodge pullout. Refer to Section 5 for design templates.
- F** Create shaded seating area near Canyon Vista Lodge and relocate existing tortoise to this location for better visibility. Include interpretive signage describing the Springdale Tortoises.
- G** Provide shade structure with bench and interpretive signage and viewing binoculars to highlight the incredible views on the east side of SR-9 north of Dixie Lane.
- H** Enhance existing seating area with shade structure, additional landscaping, and interpretive signs encouraging exploration of George Barker River Park. Provide additional amenity area just north of the park driveway that provides a river overlook and seating shaded by the existing trees.

## SHUTTLE STOP IMPROVEMENTS

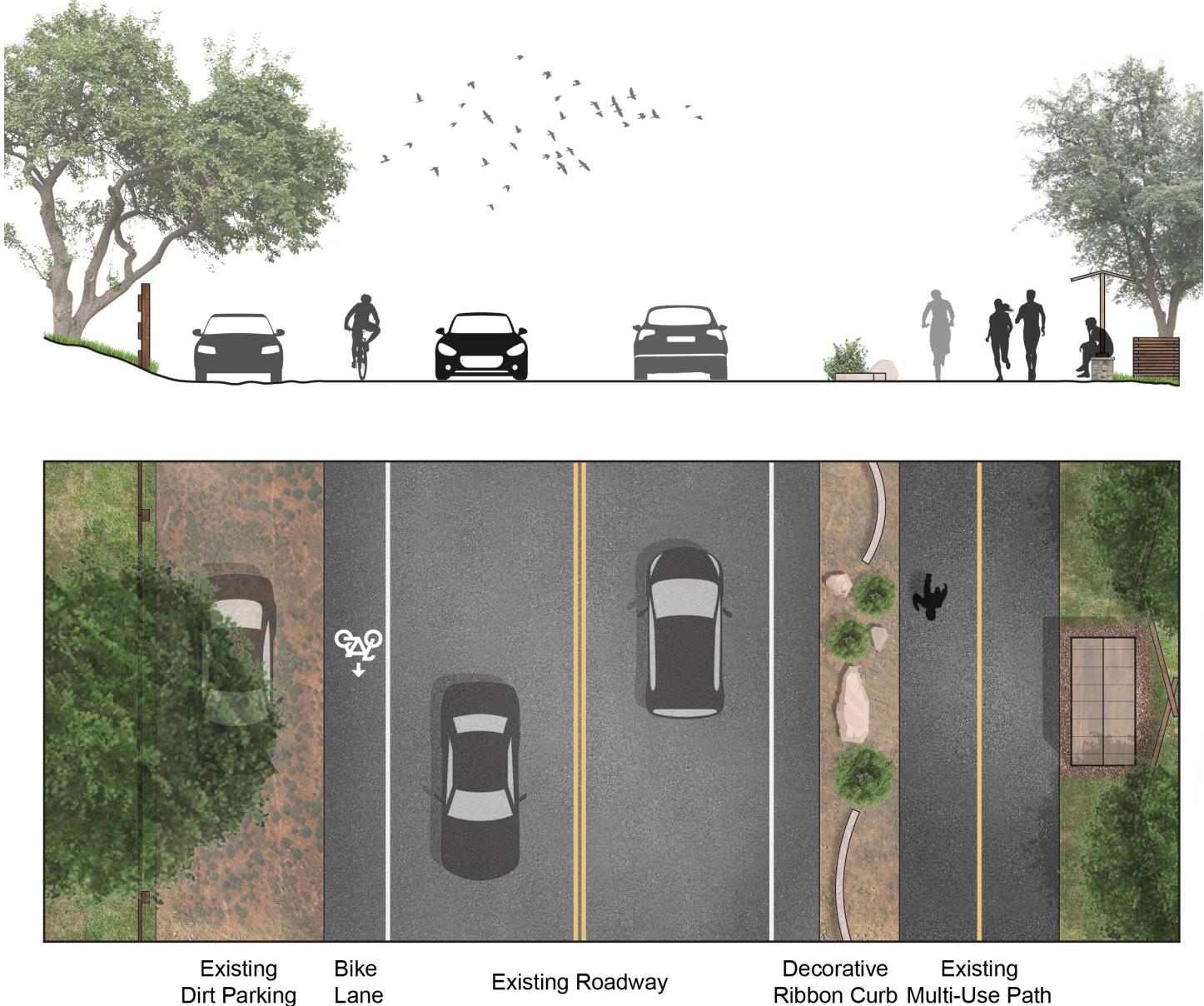
- Add shade structure and amenities as described in the Section 5 Shuttle Stop design template to shuttle stops 9 and 8 on the east side of SR-9. Add stone bench and trash receptacle to shuttle stop 8 on the west side of SR-9.

## OTHER IMPROVEMENTS

- Provide artwork on existing electrical boxes where indicated.



# STREETSCAPE PLAN



*Typical Rural Segment Section-Plan*

The section and plan view above depict the typical arrangement of the recommended SR-9 Rural Segment streetscape. The existing multi-use pathway on the east side of the road provides a wide bicycle and pedestrian corridor accented by a shaded seating node and the decorative 6" ribbon curb. Shade structures should be simple with a traditional appearance that complements the rural feel of the area.

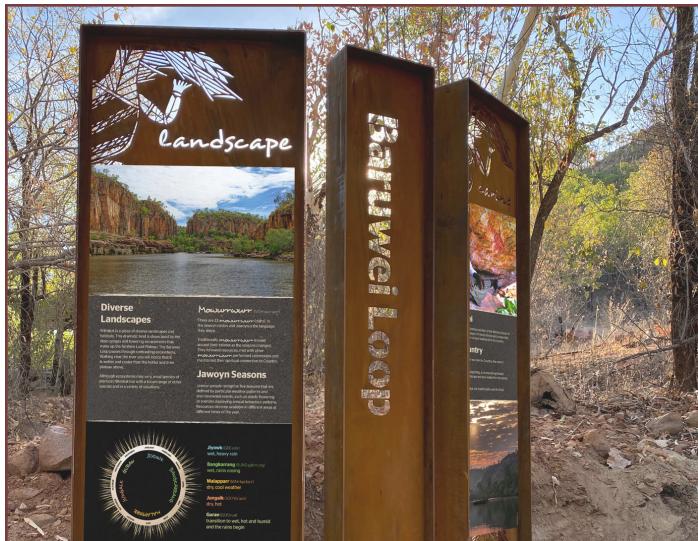
The images on page 21 reflect the overall look and character of the SR-9 Rural Segment.



*Small Shade Structure over Seating*



*Decorative 6" Ribbon Curb*



*Interpretive Signage*



*Accent Walls*



*Plantings along Pathway*



*Stone Benches*

## TRANSITIONAL SEGMENTS

The recommended streetscape treatment for the Transitional Segments includes the continued use of stone benches and accent walls in seating and enhanced landscape areas. Small, contemporary shade structures are recommended in key seating locations to provide shade and to reflect the more developed feel of the area. Ribbons of sandblasted concrete are recommended to be added to this sidewalk to continue the Self-Guided Visual Pathway to the Park. Partnership with neighboring businesses is encouraged to create informal resting nodes. Community artwork and sponsored planters are recommended to be placed near seating areas for added interest. Interpretive signs and interactive elements are also proposed.

The following is a list of recommended improvements. Refer also to the maps on pages 23 and 24.

## SAFETY IMPROVEMENTS

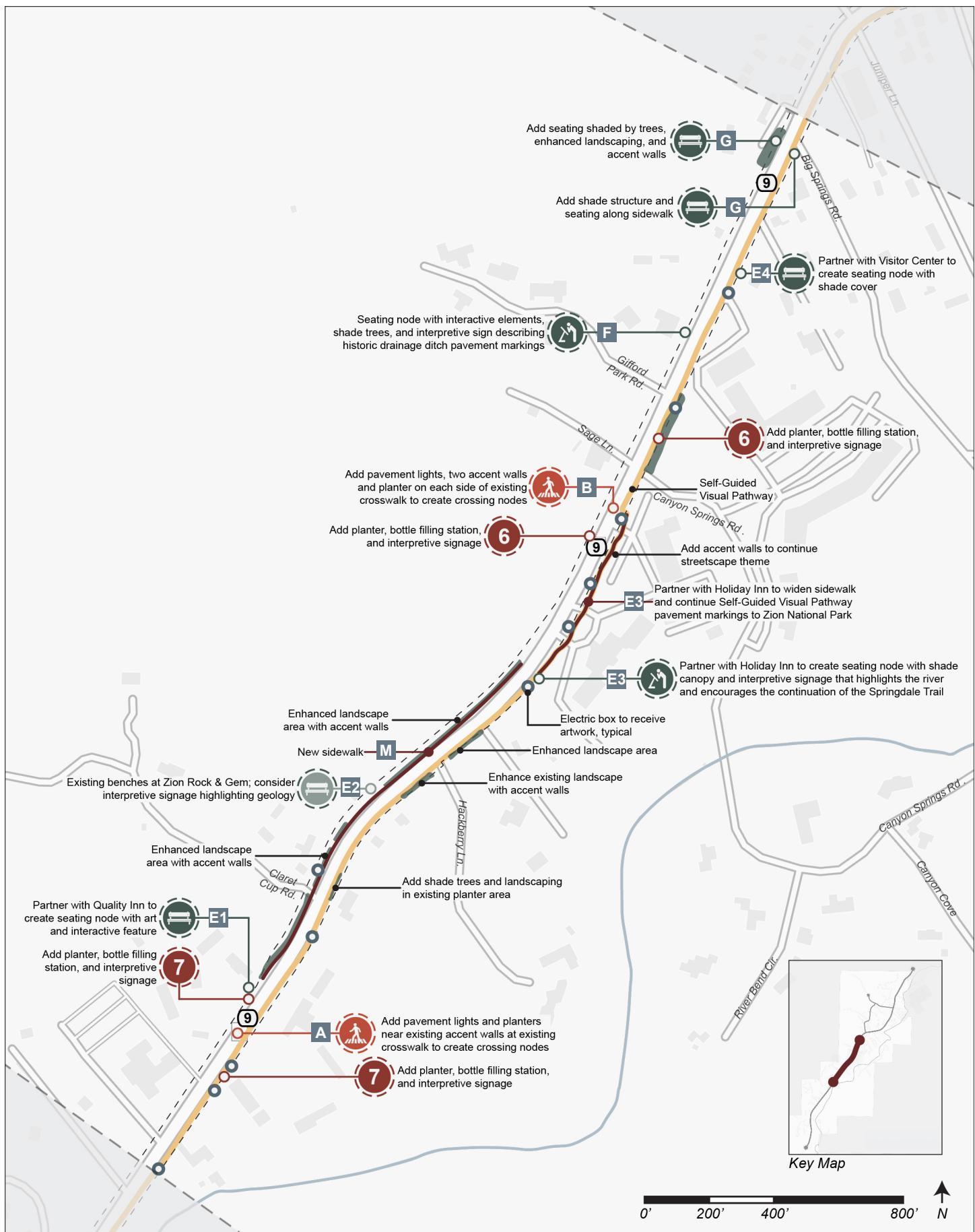
- A** Upgrade existing crosswalk near Driftwood Lodge with in-ground lights and crossing nodes per Crossing Node design template.
- B** Upgrade existing crosswalk south of Canyon Springs Road with in-ground lights and crossing nodes per Crossing Node design template.
- C** Remove existing crosswalk north of Park Lane. Install new stamped concrete crosswalk north of Hummingbird Lane with in-ground lights and crossing nodes per Crossing Node design template.
- D** Relocate existing crosswalk that is north of the Lion Boulevard intersection with SR-9 to south of the intersection. Include in-ground lights and crossing nodes per Crossing Node design template.
- Provide pedestrian-scale bollard lighting where feasible.

## PUBLIC/PRIVATE PARTNERSHIPS

- Consider partnering with the following businesses to create amenity areas featuring shaded seating, interpretive areas, interactive art features, and publicly accessible lounge seating:
  - E1** *Quality Inn and Suites Montclair* to create seating node with interactive art feature.
  - E2** *Zion Rock and Gem* to add interpretive sign near existing seating area.
  - E3** *Holiday Inn Express* to create seating node with shade structure. Partnership is needed to continue the Self-Guided Visual Pathway through Holiday Inn Express property.
  - E4** *Springdale Visitor Center* to create seating node with shade structure.
  - E5** *Hoodoos General Store* to provide a publicly accessible lounge seating area in existing lawn along with a shaded bench.
  - E6** *Flanigan's Resort and Spa* and *Zion Adventures* to provide a publicly accessible lounge seating area in existing lawn with a shaded bench.

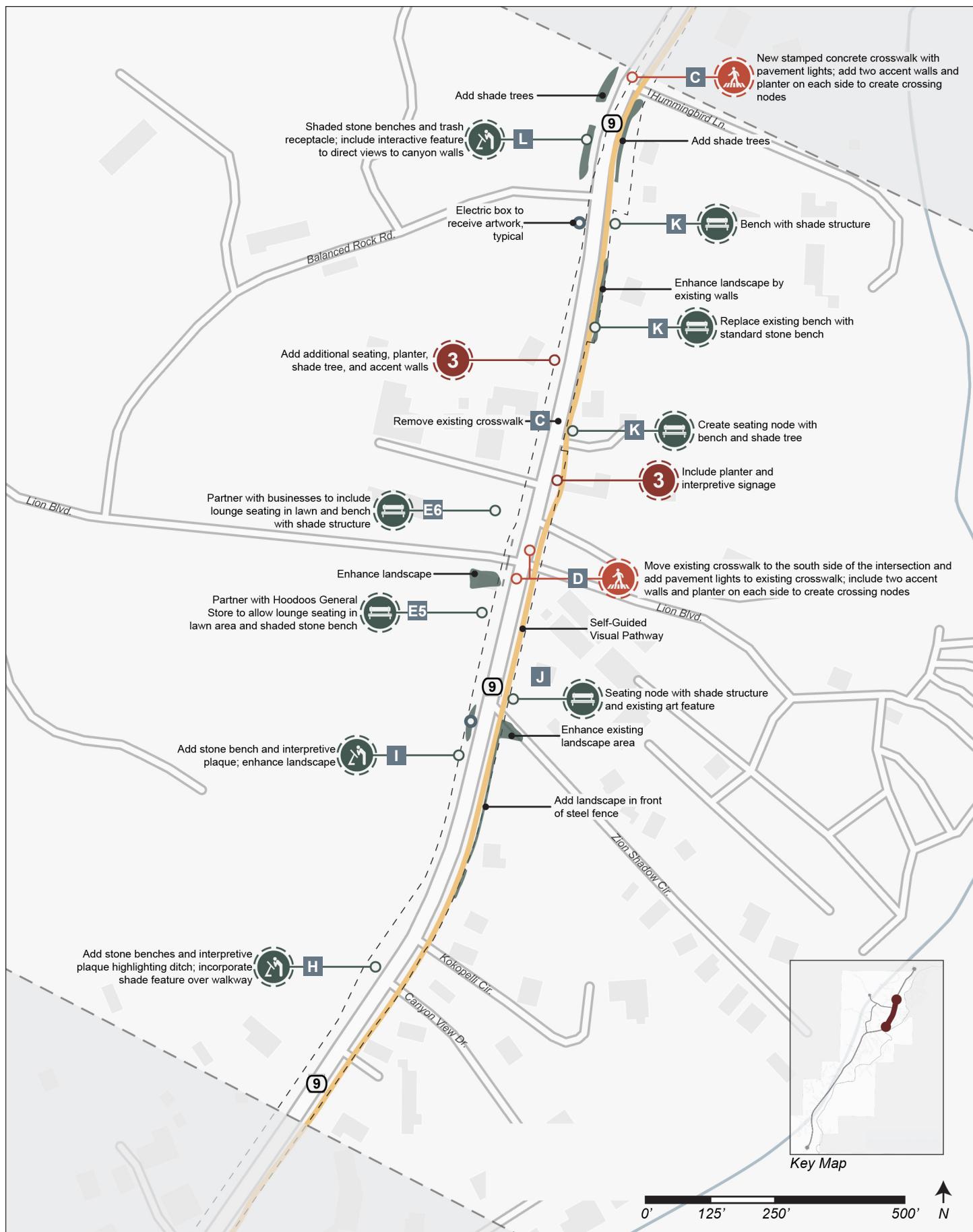
## ACTIVITY NODES

- F** Create shaded seating area near the Fire Station with interpretive sign describing the historic ditch pavement markings.
- G** Create shaded seating areas on both sides of SR-9 at the intersection with Big Springs Road.
- H** Create shaded seating area on west side of SR-9 across from Canyon View Drive with interpretive sign describing the historic ditch pavement markings.
- I** Create seating area with interpretive plaque commemorating the remnants of the historic ditch on the west side of SR-9 just south of Zion Shadow Circle.



## *Transitional Segment Map - South*

# STREETSCAPE PLAN



Transitional Segment Map - North



## ACTIVITY NODES (CONT.)

- J** Provide amenity area with seating near existing art feature on east side of SR-9 on the north side of Zion Shadow Circle intersection.
- K** Create new shaded seating areas and enhance the existing seating area with additional amenities on the east side of SR-9 south of Hummingbird Lane.
- L** Provide shaded seating area and interpretive signage with viewing binoculars to highlight the incredible views of the canyon walls on the west side of SR-9 north of Balanced Rock Road.

## SHUTTLE STOP IMPROVEMENTS

- Add amenities to shuttle stops 7 and 6 on both sides of SR-9 per Shuttle Stop design template.
- Enhance shuttle stop 3 on the east side of SR-9 by adding planters and interpretive signage.
- Provide seating, planter, shade trees, and accent walls at shuttle stop 3 on the west side of SR-9.

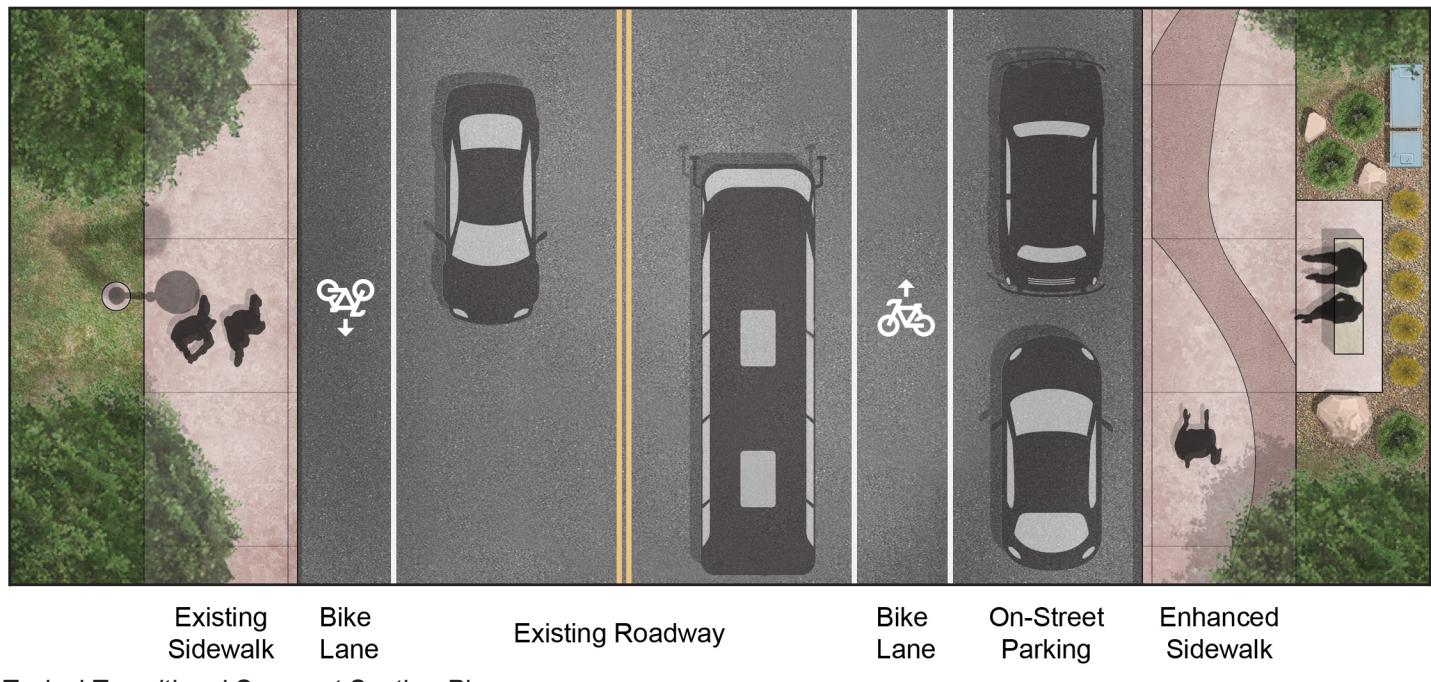
## OTHER IMPROVEMENTS

- Provide enhanced landscape in multiple areas with accent walls where indicated on maps.
- M** Construct new sidewalk on west side of SR-9 where indicated on map to enhance connectivity.
- Provide artwork on existing electrical boxes where indicated.



*Existing Sidewalk at Holiday Inn Property that Could be Converted to Self-Guided Visual Pathway*

# STREETSCAPE PLAN



*Typical Transitional Segment Section-Plan*

The section and plan view above depict the typical arrangement of the recommended SR-9 Transitional Segment streetscape. The existing sidewalk is enhanced through sand-blasted patterning on the east side of the road. Where feasible, sidewalk should be provided on both sides of the road. Activity nodes are encouraged, with shade structures being utilized in areas that do not have adjacency to existing shade trees.

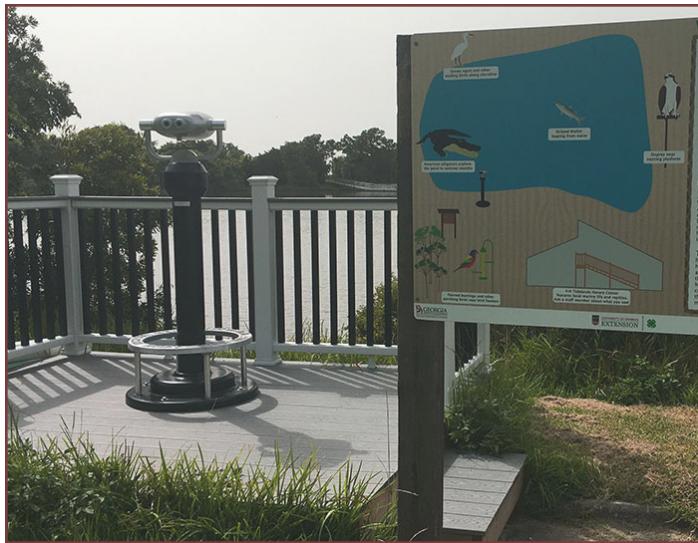
The images on page 27 reflect the overall look and character of the SR-9 Transitional Segments.



*Small Shade Structure over Seating*



*Informal Resting Nodes*



*Interpretive Signage with Interactive Features*



*Accent Walls and Enhanced Landscape*



*Sandblasted Pattern in Pavement*



*Stone Benches*

## DOWNTOWN SEGMENT

The recommended streetscape treatment for the Downtown Segment builds on the previous SR-9 segments by continuing the use of stone benches in seating areas along with small, contemporary shade structures where appropriate. The continuation of the sandblasted concrete patterning is recommended on the south side of the road to guide visitors towards the pedestrian entrance to Zion National Park. Partnership with adjacent businesses is strongly recommended to create a series of seating, dining, and interactive areas beyond the existing SR-9 right-of-way. Community artwork and sponsored planters are recommended near seating areas for added interest, along with interpretive signage and interactive elements to highlight the beautiful surroundings.

The following is a list of recommended improvements. Refer also to the map on pages 30 and 31.

### SAFETY IMPROVEMENTS

- A** Upgrade existing crosswalk at Winderland Lane to include in-ground pavement lights and crossing nodes per design template.
- B** Upgrade existing crosswalk near La Quinta Inn to include in-ground pavement lights and crossing nodes per design template.
- C** Install new stamped concrete crosswalk at the SR-9 property with in-ground lights and crossing nodes per design template.
- D** Repair existing historic ditch edge where indicated on the plan to address potential tripping hazard. Convert remnant historic ditch to planting area and add stamped pavement markings at ditch edge to connect the markings to the ditch location.
- Provide pedestrian-scale bollard lighting where feasible.

### PUBLIC/PRIVATE PARTNERSHIPS

- Consider partnering with the following businesses to create amenity areas featuring outdoor dining, shaded seating, and publicly accessible lounge seating:
  - E1** *Sol Foods Hardware* to enhance existing seating with landscaping and trash receptacle.
  - E2** *Sol Foods Supermarket* to enhance existing outdoor dining with decorative barrier, planters, and stone site furnishings.
  - E3** *MeMe's Cafe* to create a publicly accessible lounge seating area in existing lawn shaded by the large existing trees.
  - E4** *Oscar's Cafe* to create new outdoor dining area with potential to include stone site furnishings and accent walls.
  - E5** *Bumbleberry Gift Shop and Bakery* to enhance existing outdoor dining and seating areas by adding shade.
  - E6** *Zion Pizza and Noodle* to enhance existing outdoor seating area with stone site furnishings and more shade.
  - E7** *La Quinta Inn* to provide publicly accessible lounge seating area in existing shaded lawn.
  - E8** *Desert Pearl Inn* to provide publicly accessible lounge seating area in existing shaded lawn.
- F** Partner with Springdale Elementary School to provide shaded seating and interactive art features for kids to enjoy along the northwest side of SR-9.
- G** Consider partnering with the property owner of the former Fatali Gallery to add seating to existing art plaza.



## ACTIVITY NODES

- H** Create interpretive area with shaded seating and more detailed signage describing historic ditch pavement markings near MeMe's Cafe. Interpretive area to incorporate existing interpretive sign.
- I** Provide seating area on north side of SR-9 near La Quinta Inn.
- J** Provide shaded seating area on south side of SR-9 near Worthington Gallery.
- K** Upgrade existing seating area on southeast side of SR-9 near Elm Street with amenities and enhanced landscape.
- L** Create destination node at Town-owned SR-9 property. Refer to Section 6 for design concepts.

## SHUTTLE STOP IMPROVEMENTS

- Add amenities to shuttle stop 5 on both sides of SR-9 per Shuttle Stop design template.
- Add misting station and additional amenities to shuttle stop 4 on southeast side of SR-9. Add stone seating and a trash receptacle at shuttle stop 4 on northwest side of SR-9.

## OTHER IMPROVEMENTS

- Provide enhanced landscape with accent walls as noted on the plan.
- Provide new planters where noted on the plan.

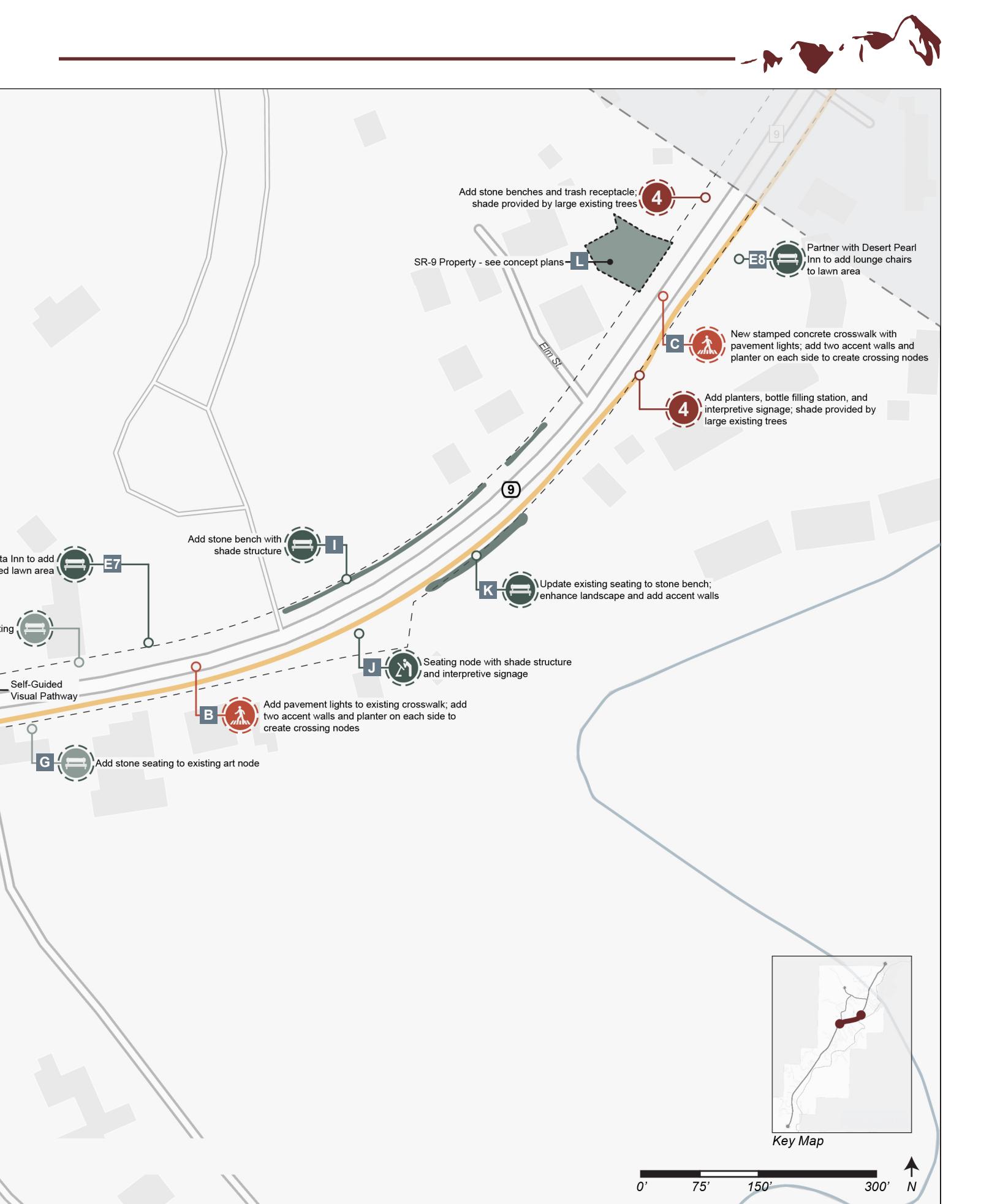


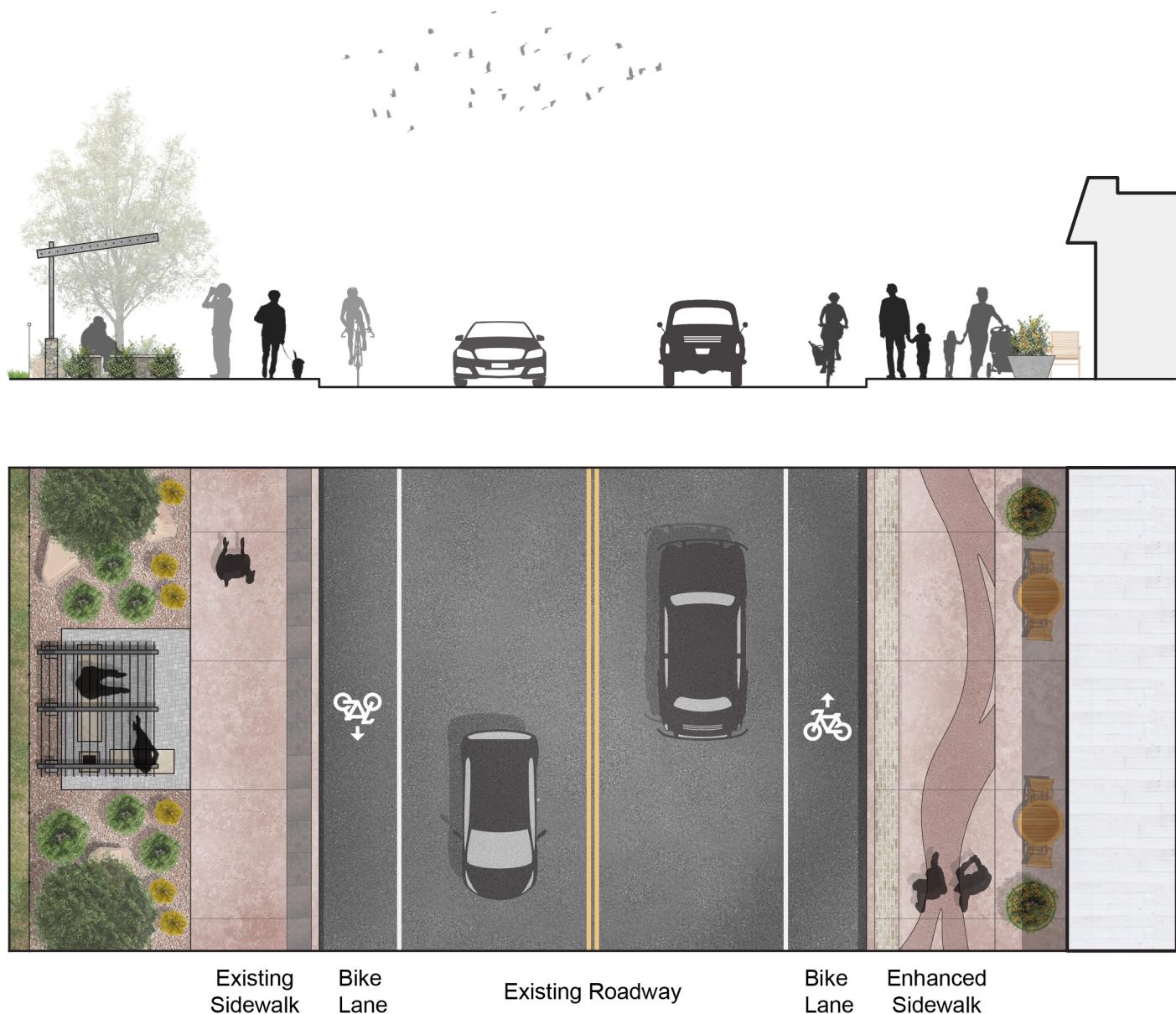
*Location of Existing Interpretive Sign Describing Historic Ditch near MeMe's Cafe*

# STREETSCAPE PLAN



Downtown Segment Map





*Typical Downtown Segment Section-Plan*

The section and plan view above depict the typical arrangement of the recommended SR-9 Downtown Segment streetscape. Continuous sidewalk exists on both sides of the road, and often features decorative patterning that highlights the historic location of the irrigation ditches. The east sidewalk is further enhanced through sand-blasted patterning. Partnership with adjacent businesses or property owners is encouraged to allow for more outdoor dining and gathering spaces beyond the limited right-of-way.

The images on page 33 reflect the overall look and character of the SR-9 Downtown Segment.



*Small Shade Structure over Seating*



*Enhanced Landscape with Art Features*



*Outdoor Dining & Gathering Spaces*



*Interpretive Signage & Interactive Art*



*Sandblasted Pattern in Pavement*



*Stone Benches*

## NORTHERN SEGMENT

Several improvements are recommended to enhance the streetscape character of the SR-9 Northern Segment. A new entry feature set into the rocky hill slope is recommended to announce arrival into Springdale. Through partnership with Zion Canyon Village, the continuation of the sandblasted and stained concrete patterning within the sidewalk is proposed to guide visitors to the Park's pedestrian entrance. Shaded seating areas with stone benches and community art provide points of interest along the way. Accent plantings, shade trees, and the continued use of stone accent walls are recommended to promote streetscape consistency throughout the entire Springdale SR-9 corridor.

The following is a list of recommended improvements. Refer also to the map on page 35.

### SAFETY IMPROVEMENTS

- A** Provide new crosswalk with Rectangular Rapid-Flashing Beacon (RRFB) with crossing nodes per design template near Zion Canyon Village's northernmost entry driveway.
- Provide pedestrian-scale bollard lighting at activity nodes where feasible.

### PUBLIC/PRIVATE PARTNERSHIPS

- B** Partner with **Zion Canyon Village** to continue Self-Guided Visual Pathway through their property to the entrance of Zion National Park. Provide additional shaded seating along the route.
- C** Consider partnering with **Cafe Soleil** to further enhance recommended activity node in area.

### ACTIVITY NODES

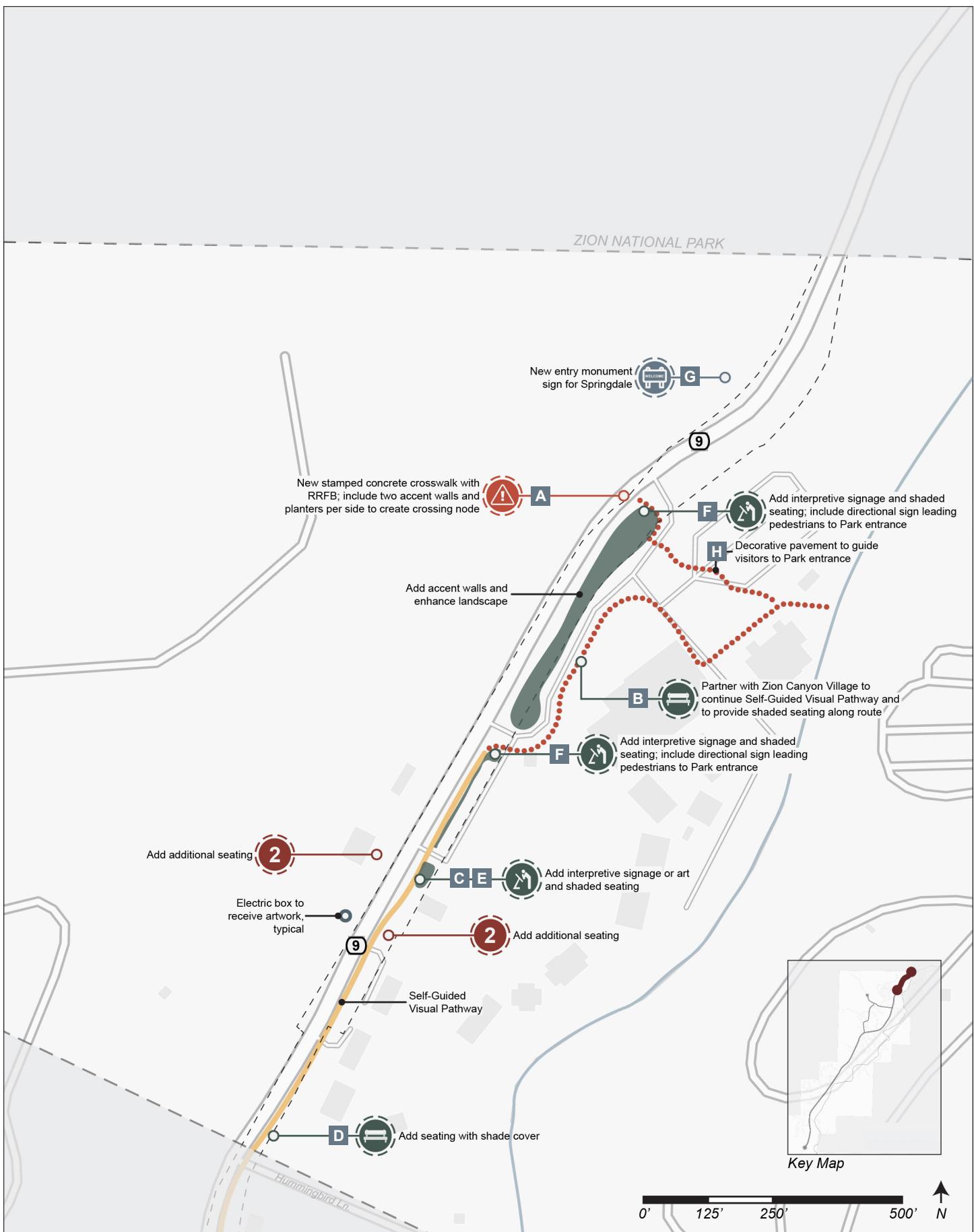
- D** Create small shaded seating area north of Hummingbird Lane.
- E** Create shaded seating area with amenities near Cafe Soleil.
- F** Create shaded seating areas near the north and south driveways for Zion Canyon Village to help guide visitors to the National Park.

### SHUTTLE STOP IMPROVEMENTS

- Add additional seating to shuttle stop 2 on both sides of SR-9.

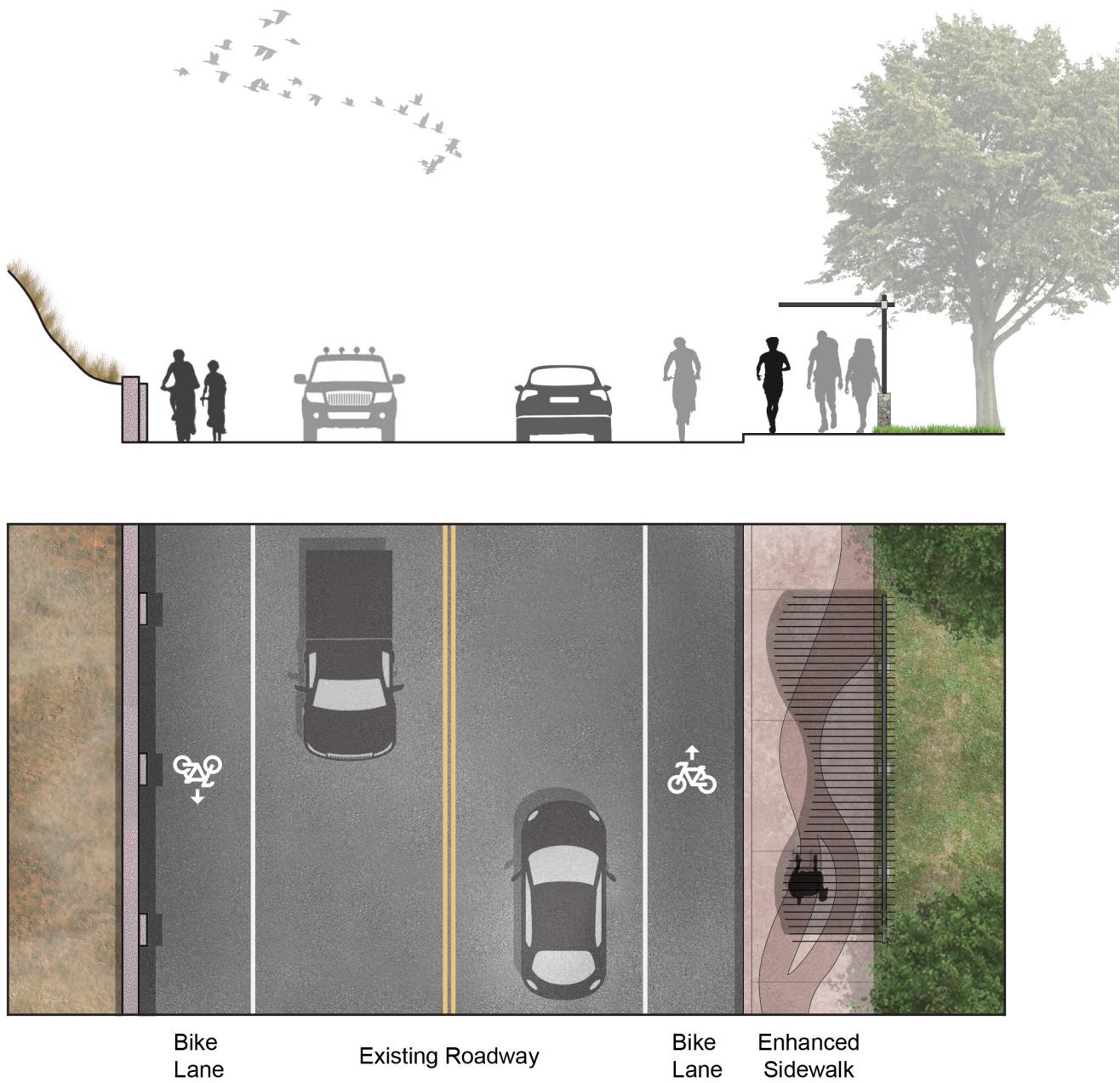
### OTHER IMPROVEMENTS

- Provide artwork on existing electrical boxes where indicated.
- Provide enhanced landscaping and accent walls where indicated on the map.
- G** Construct new entry monument that welcomes visitors to Springdale as they enter from the National Park. Detailed design and coordination with Utah's Department of Transportation will be required prior to implementation.
- H** Provide high-contrast concrete stain in conjunction with sandblasted texture to continue the Self-Guided Visual Pathway through Zion Canyon Village to the National Park's pedestrian entrance. Pavement markings should complement the design of the Self-guided Visual Pathway design template provided in Section 5 but should have a higher contrast between the river pattern and the adjacent sidewalk to clearly delineate the route to the pedestrian entrance through this commercial area.



Northern Segment Map

# STREETSCAPE PLAN



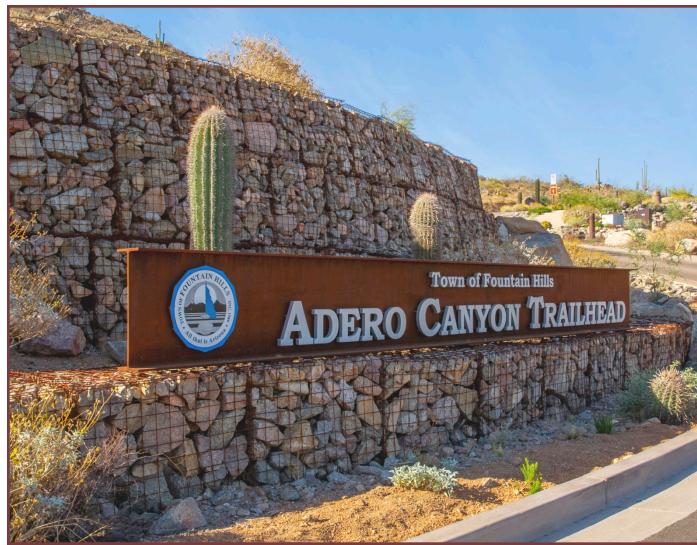
*Typical Northern Segment Section-Plan*

The section and plan view above depict the typical arrangement of the recommended SR-9 Northern Segment streetscape. The east sidewalk is enhanced through sand-blasted patterning and amenity nodes. Shade is generally lacking within this portion of the corridor. As such, shade structures could be provided over the walkways in seating node areas to shade both the seating areas and the passing pedestrians. Shade trees are also recommended where feasible.

The images on page 37 reflect the overall look and character of the SR-9 Northern Segment.



*Small Shade Structure over Walkway*



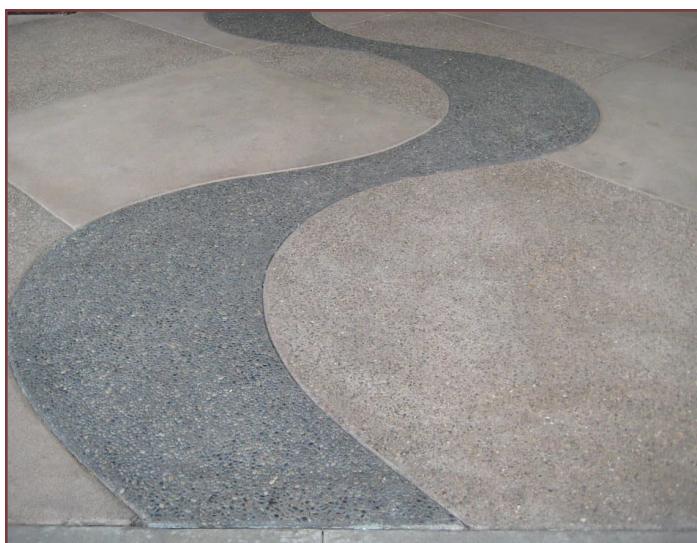
*New Entry Feature Set into Rock*



*Shade Trees*



*Existing Retaining Walls*



*Sandblasted & Stained Pattern in Pavement*



*Stone Benches*

## LION BOULEVARD

The streetscape design for Lion Boulevard builds off of the Lion Boulevard Mosaics project and encourages mosaic tilework in the recommended seating areas. Small shade structures are proposed in key locations as well as additional tree plantings that continue the existing landscape design throughout the corridor. Low-level pedestrian bollard lights are recommended near seating areas to provide additional illumination in the evening hours. Interpretive signage highlighting the history of Springdale is recommended in seating areas to contribute to the Historic Springdale Walking Tour. This signage will provide interest for both residents and visitors. Coordination with the Springdale Historic Preservation Committee is recommended for sign design, content, and placement.

The following is a list of recommended improvements. Refer also to the map on page 39.

### ACTIVITY NODES

- A** Create five (5) new seating areas along Lion Boulevard with varied amenities such as benches that could feature mosaic tilework, shade structures, shade trees, trash receptacles, and interpretive signs that highlight Springdale's culture and history.

### OTHER IMPROVEMENTS

- Provide shade trees where feasible to enhance streetscape.
- Consider paving unpaved parking areas on south side of Lion Boulevard up until Winderland Lane.

- B** Partner with the Springdale Historic Preservation Committee to create a 'Historic Springdale Walking Tour' that links each activity node.

## WINDERLAND LANE

Streetscape enhancements can help preserve the character of the residential areas of Winderland Lane from the spillover impacts from the commercial and civic activity centers on either end of the road. Continuous sidewalk along at least one side of the street will accommodate the higher volumes of pedestrian traffic this street experiences. Winderland Lane could also become a designated bicycle boulevard using signs and pavement markings to help discourage through trips by vehicles.

The streetscape design along Winderland Lane is intended to coordinate with the themes described along Lion Boulevard, including the potential incorporation of mosaic tilework in seating areas. Winderland Lane will also provide a continuation of the Historic Springdale Walking Tour noted along Lion Boulevard. Coordination with the Springdale Historic Preservation Committee is recommended for sign design, content, and placement. Low-level pedestrian bollard lights are recommended along the sidewalk, specifically near seating areas to provide some illumination in the evening hours.

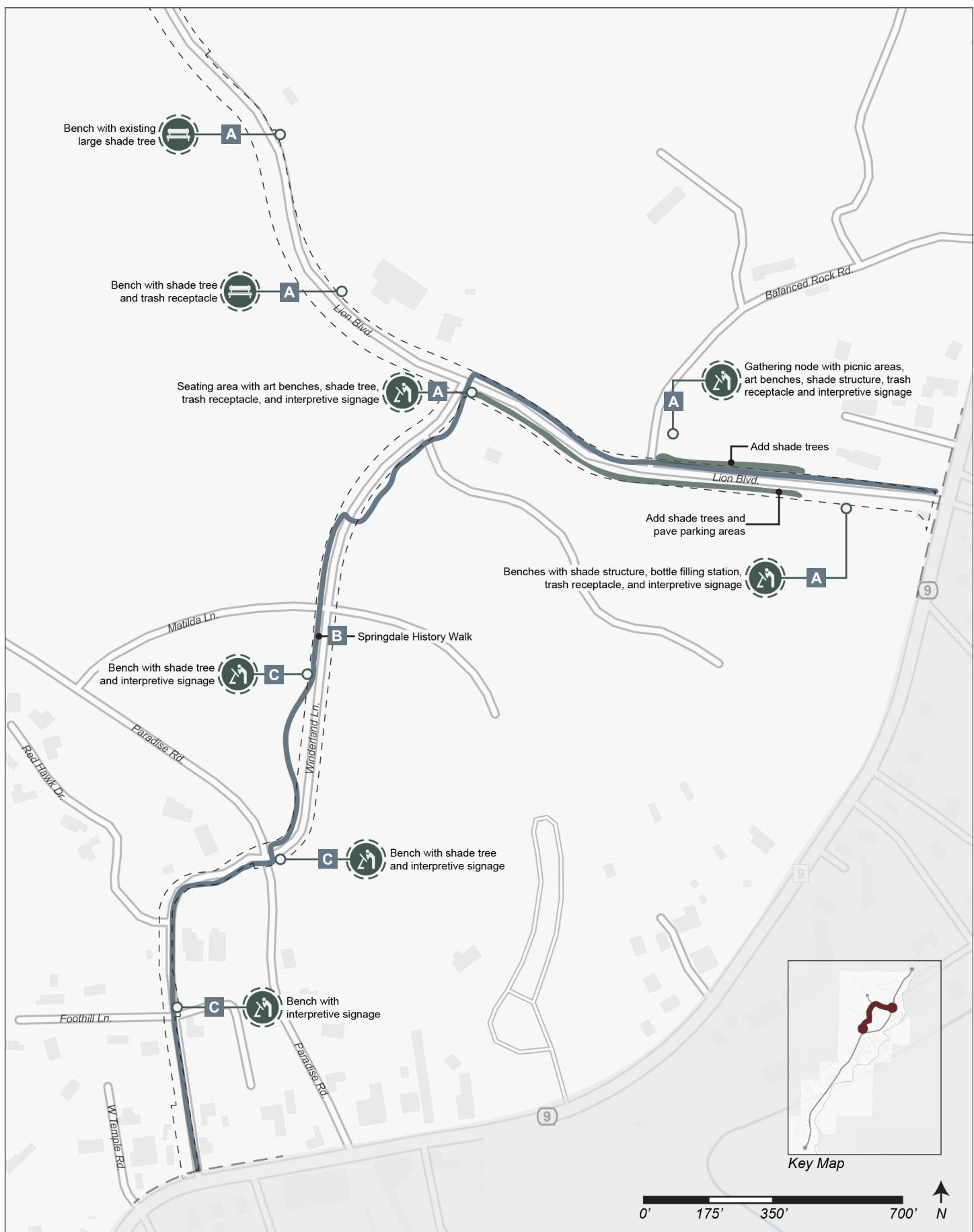
The following is a list of recommended improvements. Refer also to the map on page 39.

### ACTIVITY NODES

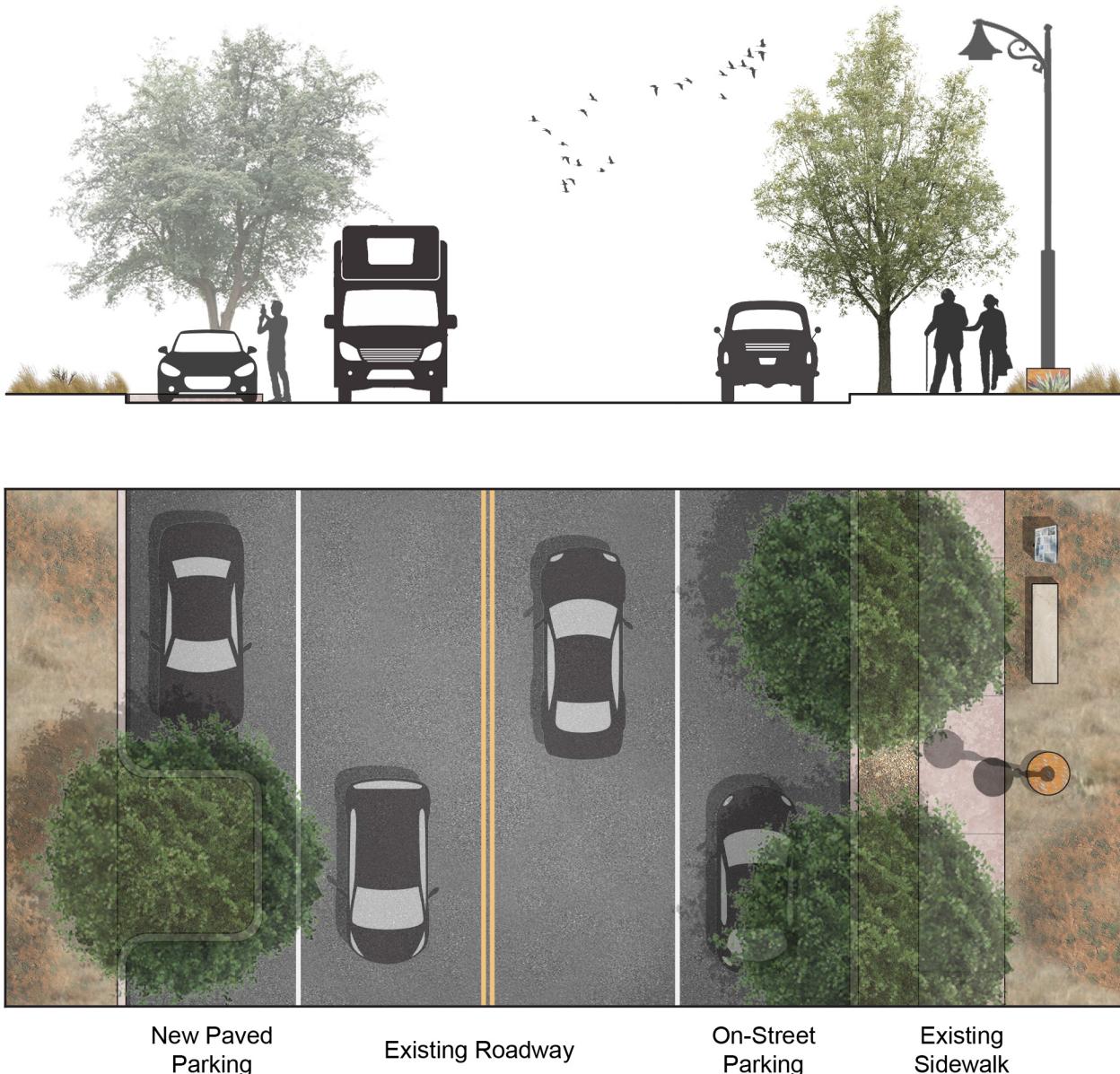
- C** Create three (3) new seating areas along Winderland Lane with benches, shade trees, and interpretive signs highlight Springdale's culture and history.

### OTHER IMPROVEMENTS

- Partner with the Springdale Historic Preservation Committee to create a 'Historic Springdale Walking Tour' that links each activity node.



Lion Boulevard and Winderlane Lane Map



*Typical Lion Boulevard Section-Plan*

The section and plan view above depict the typical arrangement of the recommended Lion Boulevard streetscape. Continuous sidewalk exists on the north side of the road along with parking on both sides of the road. It is recommended that the dirt parking areas on the south side of Lion Boulevard be paved from SR-9 up to Winderland Lane.

The images on page 41 reflect the overall look and character of the Lion Boulevard streetscape.



*Small Shade Structure over Seating*



*Street Tree Plantings*



*Interpretive Signage*



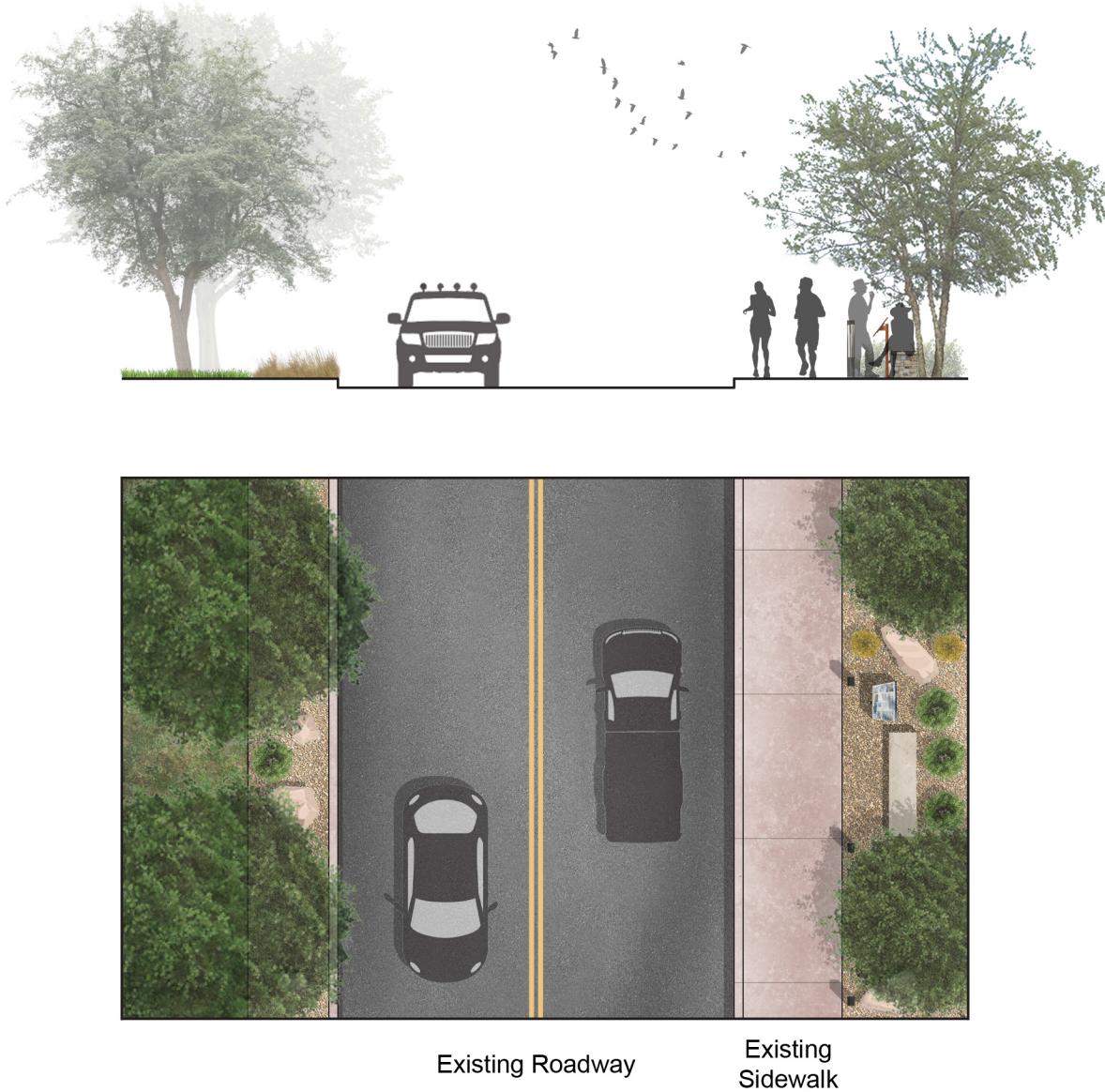
*Mosaic Light Pole Bases*



*Mosaic Accents on Utility Access Lids*



*Bench with Mosaic Tilework*



*Typical Winderland Lane Section-Plan*

The section and plan view above depict the typical arrangement of the recommended Winderland Lane streetscape. Existing sidewalk is located on the east side of Winderland Lane until the intersection with Paradise Road. North of Paradise Road, new sidewalk has recently been constructed on the west side of the road as part of a new housing development.

The images on page 43 reflect the overall look and character of the Winderland Lane streetscape.



*Accent Plantings with Boulders*



*Street Tree Plantings*



*Interpretive Signage*



*Bollard Lighting*



*Typical Concrete Sidewalk*



*Bench with Mosaic Tilework*



*“There is immense power when a group of people with similar interests gets together to work toward the same goals.”*

*Idowu Koyenikan*



## 05. DESIGN TEMPLATES

To maintain consistency in some of the key repeating design elements found throughout the corridor, several design templates have been created to guide future design and construction efforts in Springdale. This section provides guidance for creating the Self-Guided Visual Pathway, developing safer driveway areas, designing comfortable seating areas, developing safer crossings, and enhancing the existing shuttle stops. All of the ideas presented in this section are intended to be combined with the recommendations provided in the previous section. A detailed planting palette has also been provided to assist adjacent property owners in enhancing their streetscape frontages where feasible.

The design templates in this section are intended to serve as a guide for Town staff or design consultants as improvements begin to take place within the streetscape environment. The templates help to provide consistent, clear, and understandable design cues along the streetscape to aid in wayfinding and to enrich the overall user experience. Further detailed design and coordination will be required prior to implementing any of the recommended improvements.

All streetscape improvements must meet American Disabilities Act (ADA) accessibility requirements and should include universal design principles to promote a welcoming, inclusive environment. Streetscape improvements must maintain a minimum 5' accessible clear path of travel on sidewalks and provide ADA access ramps with detectable warning surfaces where needed. An ADA Transition Plan is recommended for existing streetscape features that will remain in place that do not currently meet ADA requirements.

Additional coordination with Utah's Department of Transportation is needed prior to making any modifications to existing crosswalks or prior to adding new crosswalk locations. Coordination will also be required with Zion National Park to make modifications to the existing shuttle stops. Community partnership will be critical to develop an appropriate strategy for installing and/or maintaining the recommended planters at the shuttle stop and crossing node locations.

As with any plan, community needs and desires are fluid and can change over time. There is inherent flexibility within the design templates so that they can serve as a starting point prior to moving an improvement into construction. Continued dialog with business-owners and residents is encouraged to foster community interest and support for these improvements throughout implementation.



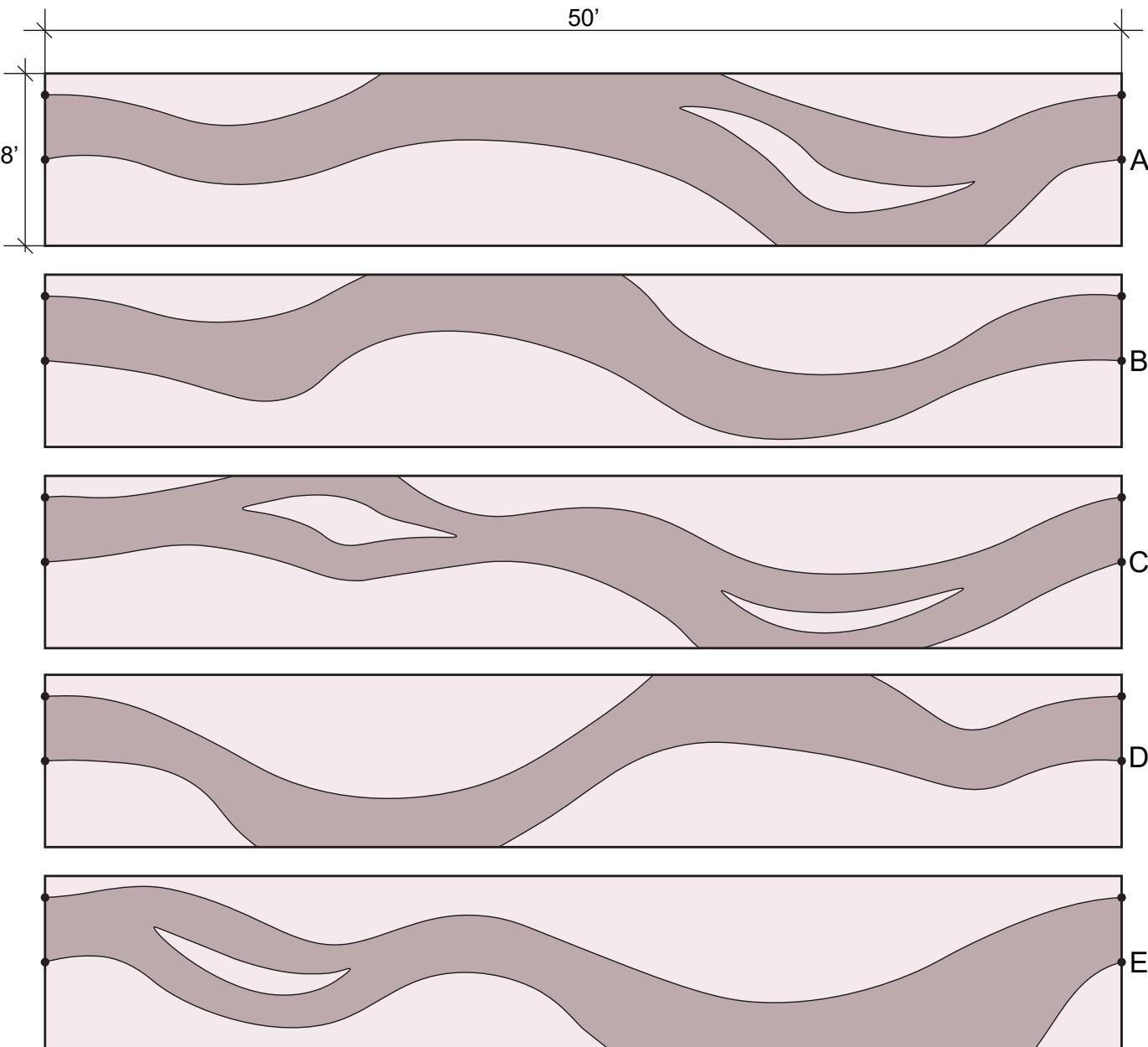
*Field Review of Potential Improvements*

### SELF-GUIDED VISUAL PATHWAY

A Self-Guided Visual Pathway leading visitors from the Majestic Lodge parking pullout to the entrance of Zion National Park is proposed along the east side of the corridor. The Self-Guided Visual Pathway takes inspiration from the adjacent Virgin River as it winds through Springdale and into the Park. Serpentine ribbons of 6" concrete curb are recommended adjacent to the existing multi-use path to create this feature, while sandblasted patterns into the existing concrete are recommended once the multi-use path transitions to existing sidewalk.

### SIDEWALK TREATMENT

The five (5) templates below should be used to sandblast the serpentine appearance of the pathway into the existing 8' wide sidewalks. The sandblasted area could be stained to further accent the ribbon, or left natural for a more subtle appearance. The pattern connects end to end and may be repeated as long as needed. Coordination with Zion Canyon Village is required to continue this



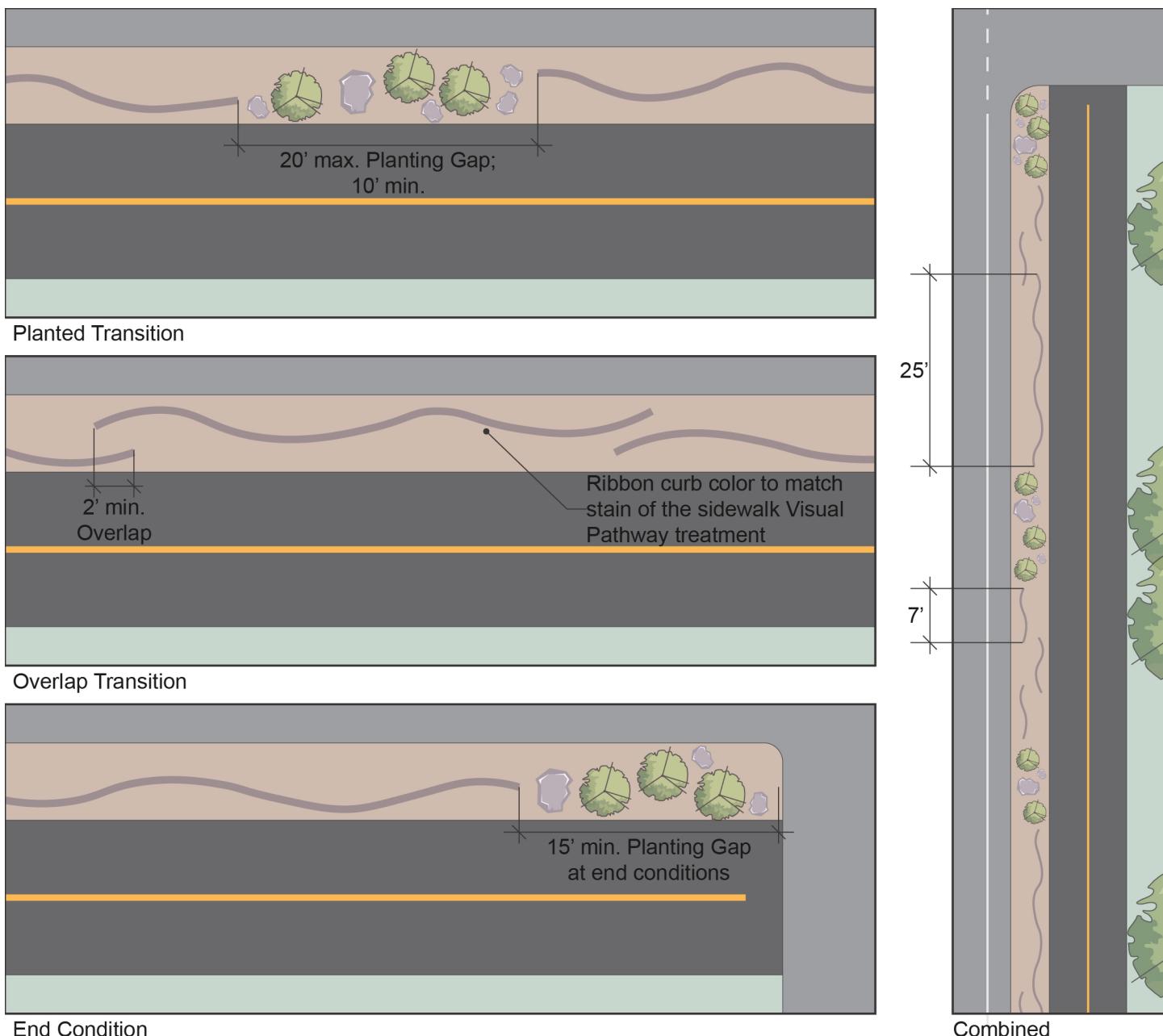
*Self-Guided Pathway: Sidewalk Treatment Template*



paving treatment through the existing sidewalks and plaza spaces towards the Zion National Park pedestrian entrance. Interpretive plaques in the pavement should be included near each shuttle stop and at the Majestic Lodge parking pullout to highlight the intent of the Self-Guided Visual Pathway.

## MULTI-USE PATHWAY TREATMENT

The images below provide the recommended design intent for the Self-Guided Visual Pathway adjacent to the existing multi-use pathway in the Rural segment of the project. As noted below, combinations of 25' and 7' concrete ribbon curb segments are recommended in varying patterns to create a serpentine appearance in the landscape. The ribbon curb is recommended to be 6" above finished grade and stained the same color as the sandblasted pattern described in the sidewalk treatment or stamped for added interest. Planting groups should be interspersed with the curbs.





*Sandblasted & Stained Pattern in Pavement*



*Sandblasted Pattern in Pavement*



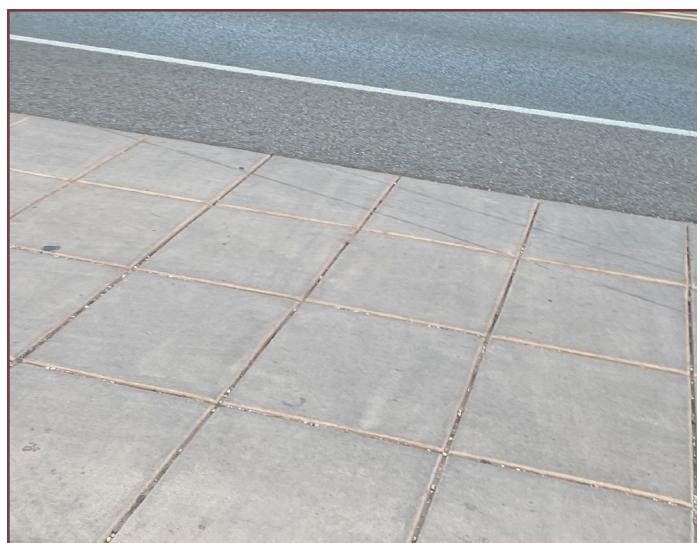
*Decorative Ribbon Curb*



*Decorative Ribbon Curb*



*Example of Interpretive Plaque in Pavement*



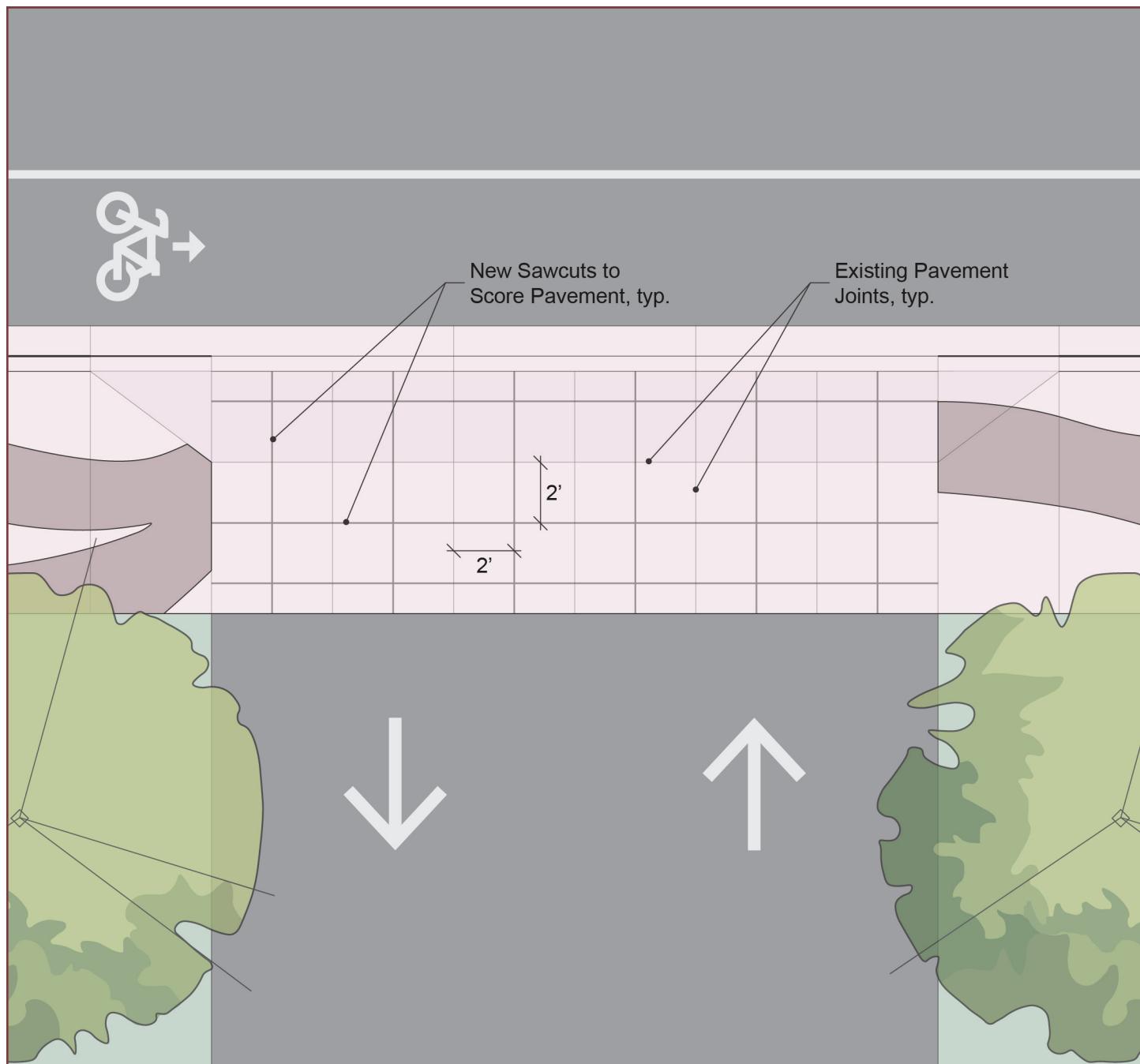
*Driveway Access Paving Treatment*



## DRIVEWAY ACCESS PAVEMENT TREATMENT

There are many driveway locations throughout the corridor that create the potential for conflicts between vehicular traffic and pedestrians. To better define these user conflict zones, additional scoring of the existing driveway aprons along SR-9 is recommended. The template below provides the intended design of these areas, including a 2' x 2' saw-cut grid set within the existing pavement.

This template is designed to work with the existing pavement joints to minimize disruption to the existing sidewalk. The textured pattern mimics the pattern used in the shuttle pullout areas to designate the shuttle area from the rest of the roadway. This repeating pattern of scored concrete, either in the shuttle pullouts or in the driveways, provides additional notice to both drivers and pedestrians that they are entering into an area that could have multiple modes of traffic.

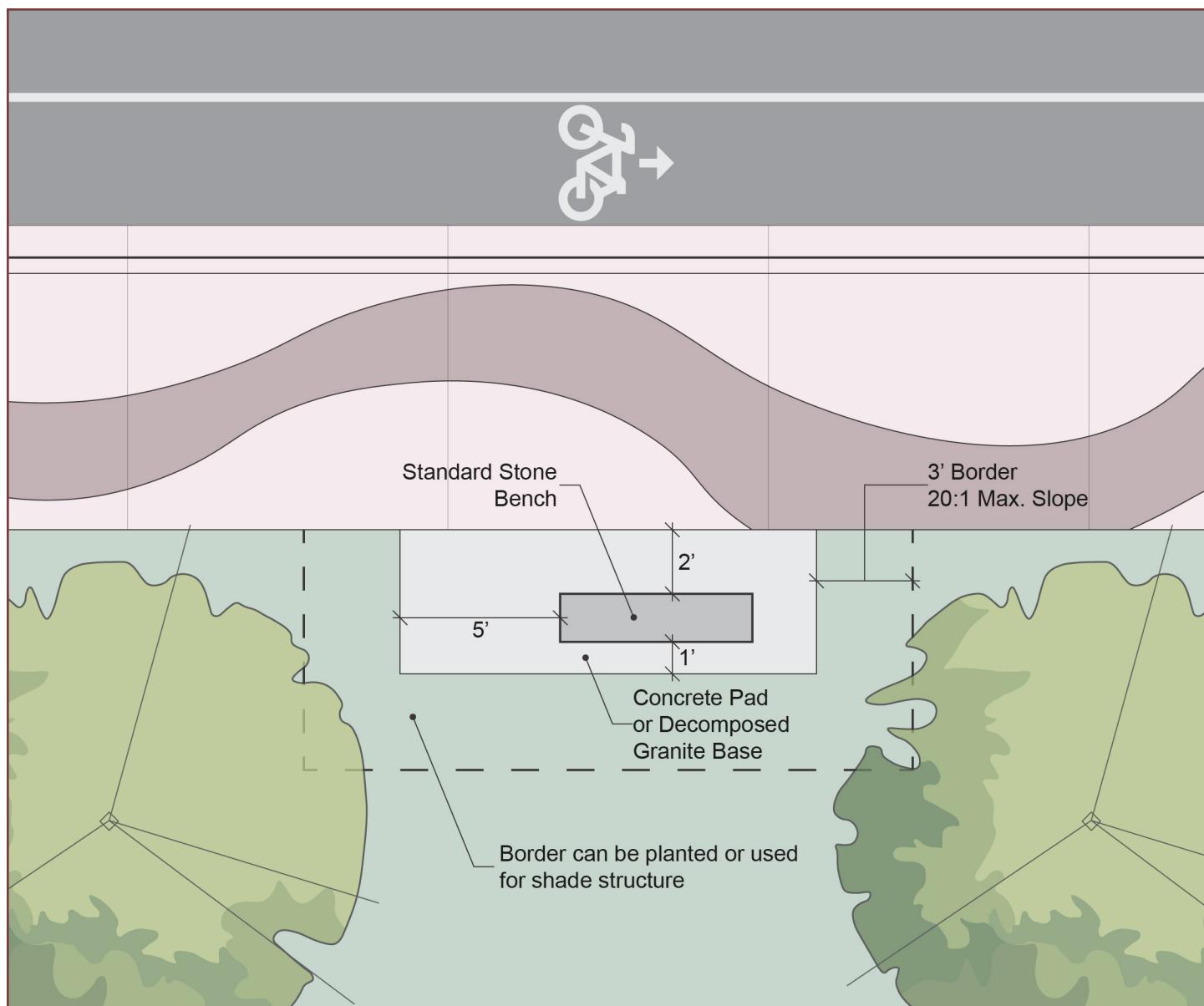


Driveway Access Treatment Template

### SEATING NODES

As noted by both stakeholders and the community, more shaded seating is needed in Springdale. The design templates provided below offer two different styles of seating nodes. The seating nodes along SR-9 should include stone benches that match the style of benches used in the majority of the shuttle stops. If the seating node is adjacent to Lion Boulevard or Winderland Lane, an art bench may be used instead. Stone trash receptacles are also recommended along SR-9 to maintain consistency. If desired along Lion Boulevard or Winderland Lane, trash receptacles could incorporate artwork to tie-in to the overall theme for those segments.

Where recommended, shade structures should reflect the style of the shade structure imagery provided in each segment of the corridor as described in Section 4. Recommended offsets have been provided in the templates below to allow for adequate separation from the adjacent sidewalk and to improve accessibility. Where feasible, shade trees or accent plantings are recommended adjacent to the seating nodes to provide added color and seasonal interest. Refer to Section 4 for a detailed description of the various amenities that could be offered at each seating node.



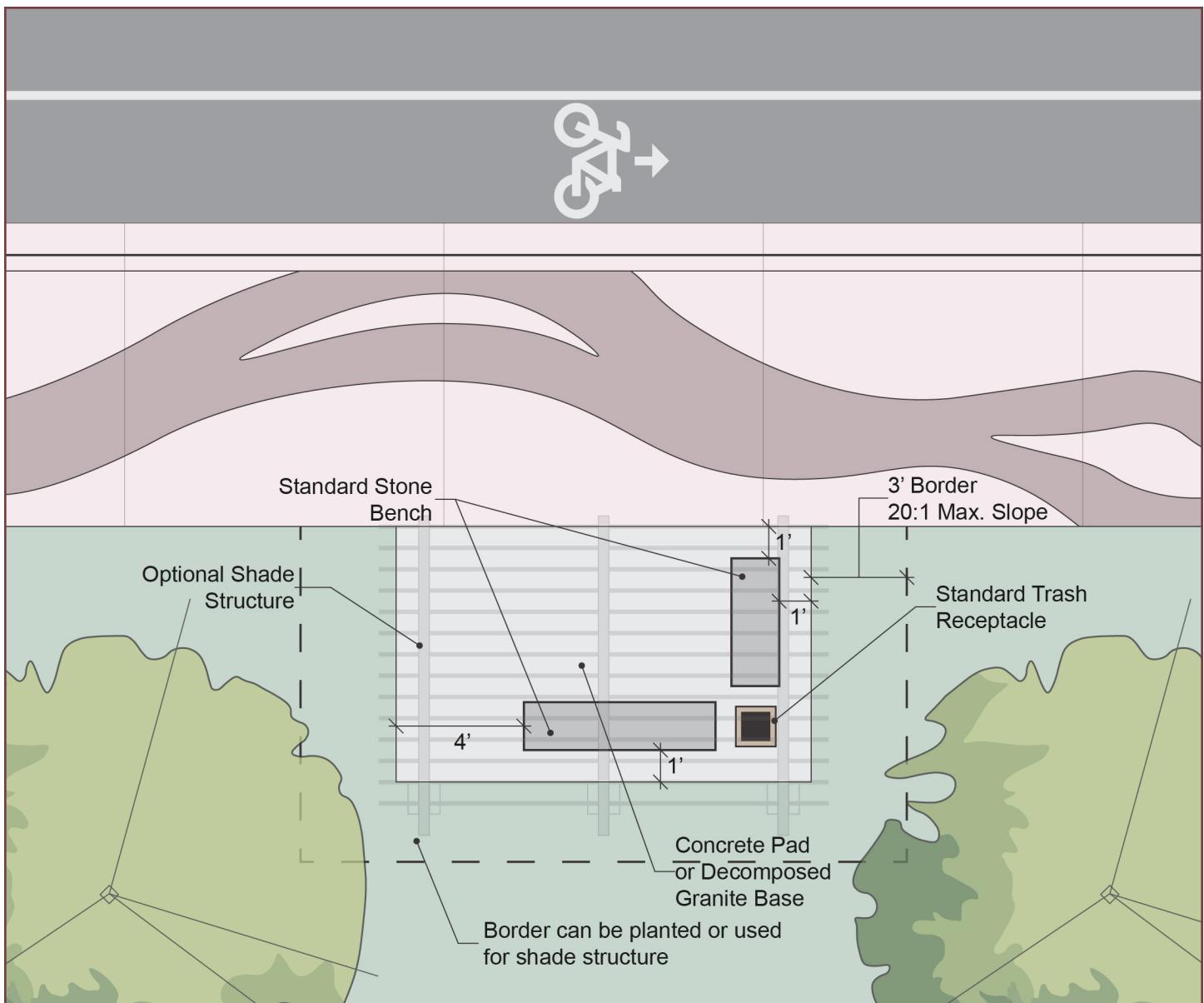
Single Bench Seating Node Minimum Offsets



Standard Stone Bench and Trash Receptacle



Potential Shade Structure



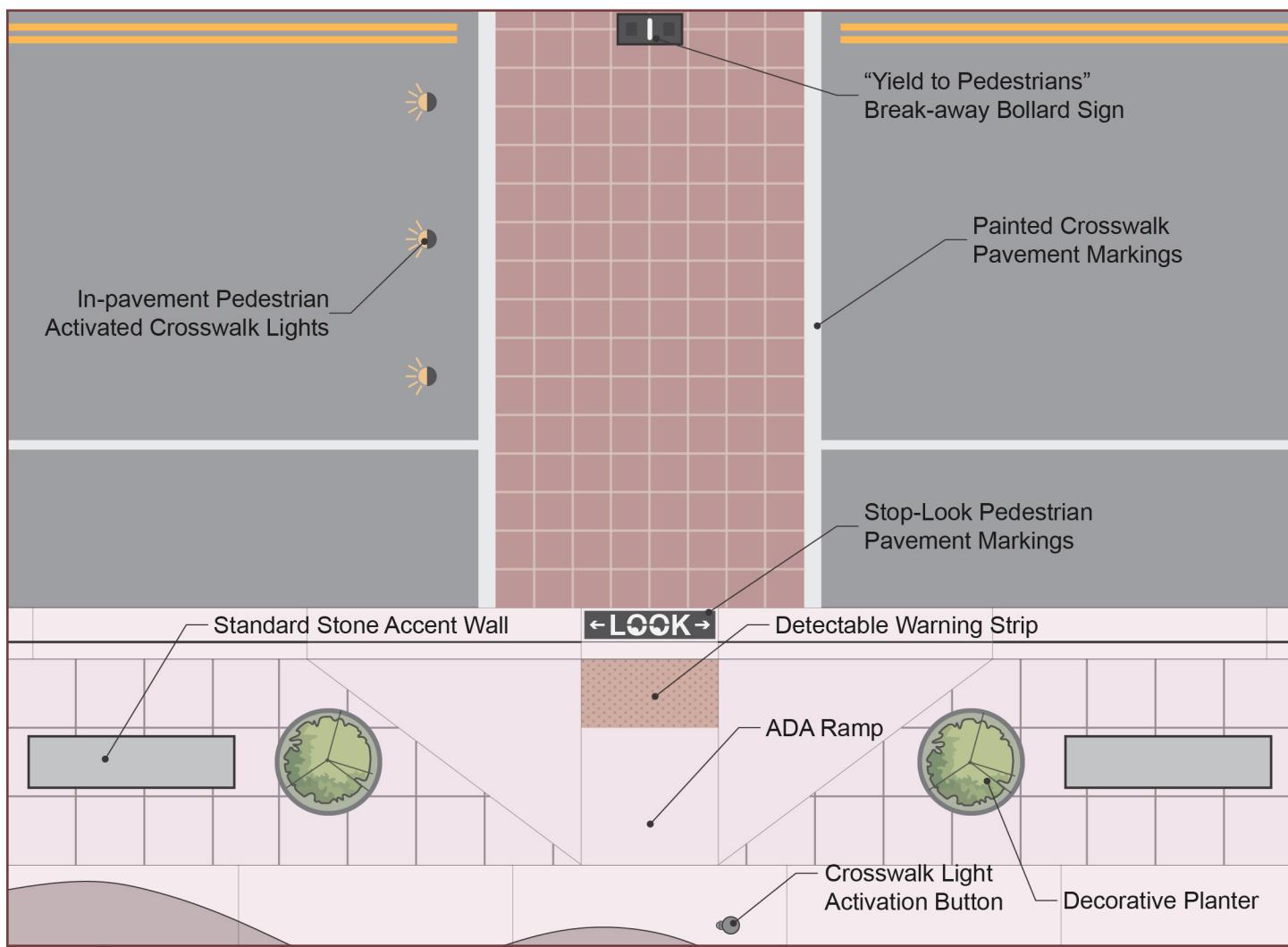
Double Bench Seating Node Minimum Offsets

### CROSSWALKS AND CROSSING NODES

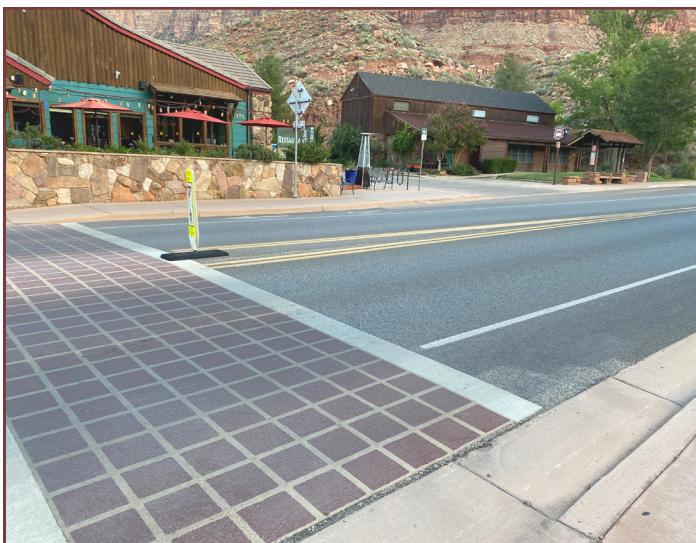
Given the high volumes of both pedestrian and vehicular traffic along SR-9, several existing crosswalks provide designated locations for pedestrians to cross the road. As recommended in the Springdale Downtown Transportation Study and Active Transportation Plan, four new crosswalk locations are proposed. Based on stakeholder and community input, several improvements are recommended for the existing and proposed crosswalks.

The existing decorative crosswalk treatment is highly desirable by the community and should be continued in the new crosswalks. However, visibility of the pavement markings can sometimes be limited due to lighting conditions. In-pavement pedestrian activated lighting is recommended to increase visibility. In-pavement lights must be installed per the requirements of the Manual on Uniform Traffic Control Devices Chapter 4N. Additional pavement markings reminding pedestrians to stop and look are also proposed. The consistent use of two stone seatwalls on either side of the crosswalks along with decorative planters create a signature crossing node at each crosswalk location.

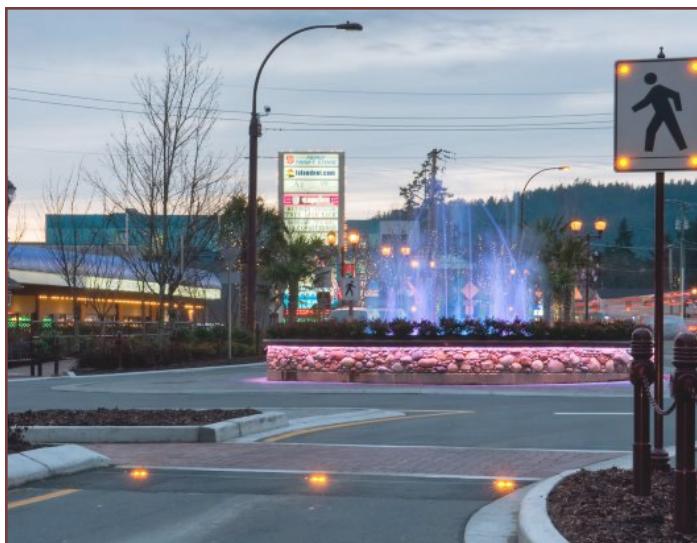
A rectangular rapid-flashing beacon (RRFB) signal is recommended at two crosswalk locations due to the speed of incoming vehicles or limited sight lines in the roadway. The installation of the RRFB signals should be further vetted by the Town to weigh enhanced safety against the need to preserve community character. Some community members have expressed concern about the potential lighting impacts of the RRFB signals.



Crossing Node Template



*Decorative Crosswalk Treatment*



*In-pavement Pedestrian Activated Lighting*



*RRFB at Select Crossings*



*Stop. Look. Pavement Markings*



*Stone Seatwalls at Each Crossing*



*Decorative Planters at Each Crossing*

### SHUTTLE STOPS

Many of the Springdale shuttle stops along SR-9 provide seating, shade, and other amenities such as water bottle filling stations, trash receptacles, and bicycle racks. The shuttle stops offer consistent seating and gathering destinations along the corridor. Based on input from the community, the appearance of the traditional, shaded shuttle stop is highly desirable and serves as one of the key components to the overall SR-9 streetscape theme. The stone seating and accent elements that are featured within the existing shuttle stops are repeated throughout much of the corridor.

Several additional amenities are recommended to further enhance the shuttle stops and to take advantage of existing, well-defined gathering nodes. Interpretive signs created in partnership with Zion National Park, directional signs, enhanced landscaping with educational plaques, and community art could also be incorporated at each shuttle stop to inform visitors of the area while adding interest. Interpretive plaques describing the intent of the Self-Guided Visual Pathway are also recommended in the pavement since the shuttle stops will likely be one of the visitor's first stops in Springdale.



*Typical Shuttle Seating & Shade Structure*



*Springdale Bicycle Racks*



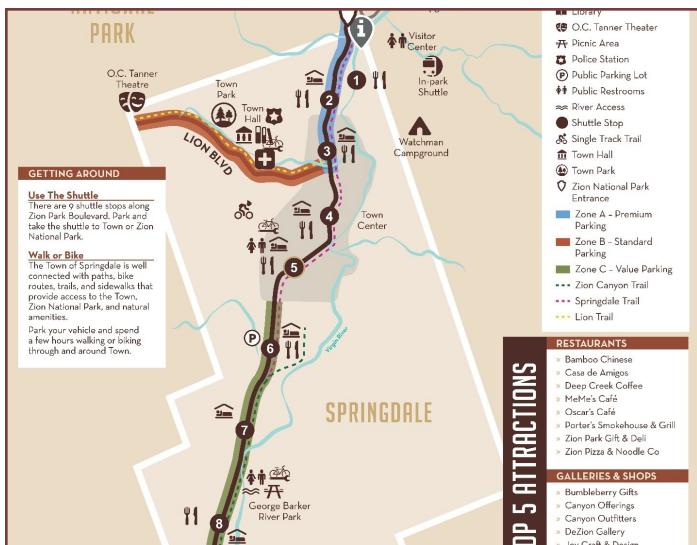
*Water Bottle Filling Station and Trash Receptacle*



*Decorative Planters*



Existing Shuttle Stop with Shade Structure and Shaded Landscape Area



Map of Community for Wayfinding



Signage Indicating Walking Distance to Park



Partnership with Zion National Park for Signage



Enhanced Landscape

## PLANT PALETTE

The following species have been selected for the Streetscape Plan due to their drought-tolerance, limited maintenance requirements, and appropriateness for the Springdale climate. Many of the species are also reported to have some resistance to deer grazing. The list of plant materials on the next several pages can fulfill many design needs and provide for canopy shade, color, bold contrast and accent, barrier, erosion control, screening, and fragrance.

### SHADE TREES

Botanical Name	Common Name	Size (H x W)	Image
<i>Acer glabrum</i>	Rocky Mountain Maple	20-30' x 10-15'	
<i>Acer negundo</i>	Boxelder	30-50' x 30-50'	
<i>Fraxinus anomala</i>	Single-leaf Ash	10-12' x 10-12'	
<i>Pinus flexilis 'Vanderwolf's Pyramid'</i>	Vanderwolf's Pyramid Limber Pine	20-25' x 10-15'	

### ORNAMENTAL TREES

Botanical Name	Common Name	Size (H x W)	Image
<i>Amelanchier utahensis</i>	Utah Serviceberry	15' x 10'	
<i>Chilopsis linearis</i>	Desert Willow	15-25' x 10-15'	
<i>Forestiera neomexicana</i>	Desert Olive	15' x 12'	
<i>Picea pungens 'Iseli Fastigiate'</i>	Columnar Blue Spruce	15' x 5'	
<i>Robinia neomexicana</i>	New Mexico Locust	10-15' x 10'	



## SHRUBS/ACCENTS

Botanical Name	Common Name	Size (H x W)	Image
<i>Agave spp.</i>	Agave Species	Varies	
<i>Asclepias tuberosa</i>	Butterfly Milkweed	2-3' x 1-2'	
<i>Aster glaucodes</i>	Glaucous Aster	3' x 2'	
<i>Datura wrightii</i>	Datura	2-3' x 3'	
<i>Encelia frutescens</i>	Brittlebush	2' x 4'	
<i>Ephedra viridis</i>	Mormon Tea	3' x 5'	
<i>Hesperaloe parviflora</i>	Red Yucca	3-5' x 4-6'	
<i>Mirabilis multiflora</i>	Four O'Clock	2-4' x 1-3'	
<i>Salvia dorrii</i>	Desert Sage	1-3' x 2-3'	
<i>Sphaeralcea spp.</i>	Globemallow Species	Varies	
<i>Stanleya pinnata</i>	Prince's Plume	3-5' x 1-2'	
<i>Yucca spp.</i>	Yucca Species	Varies	

## DESIGN TEMPLATES

### FORBS

Botanical Name	Common Name	Size (H x W)	Image
<i>Baileya multiradiata</i>	Desert Marigold	1-2' x 2'	
<i>Castilleja chromosa</i>	Common Paintbrush	0.5-1.5' x 1'	
<i>Castilleja linariifolia</i>	Linearleaf Paintbrush	0.5-1.5' x 1'	
<i>Erigeron utahensis</i>	Utah Daisy	1' x 1.5'	
<i>Gaillardia aristada</i>	Blanketflower	1-3' x 2'	
<i>Heterotheca villosa</i>	Hairy Goldenaster	1-3' x 1-2'	
<i>Linum lewisii</i>	Flax	1-2' x 1'	
<i>Penstemon spp.</i>	Penstemon Species	Varies	

### GRASSES

Botanical Name	Common Name	Size (H x W)	Image
<i>Aristida purpurea</i>	Purple Threeawn	1-3.5' x 2'	
<i>Elymus elymoides</i>	Squirrel Tail Grass	2' x 2'	
<i>Schizachyrium scoparium</i>	Little Blue Stem	2-3' x 1-2'	

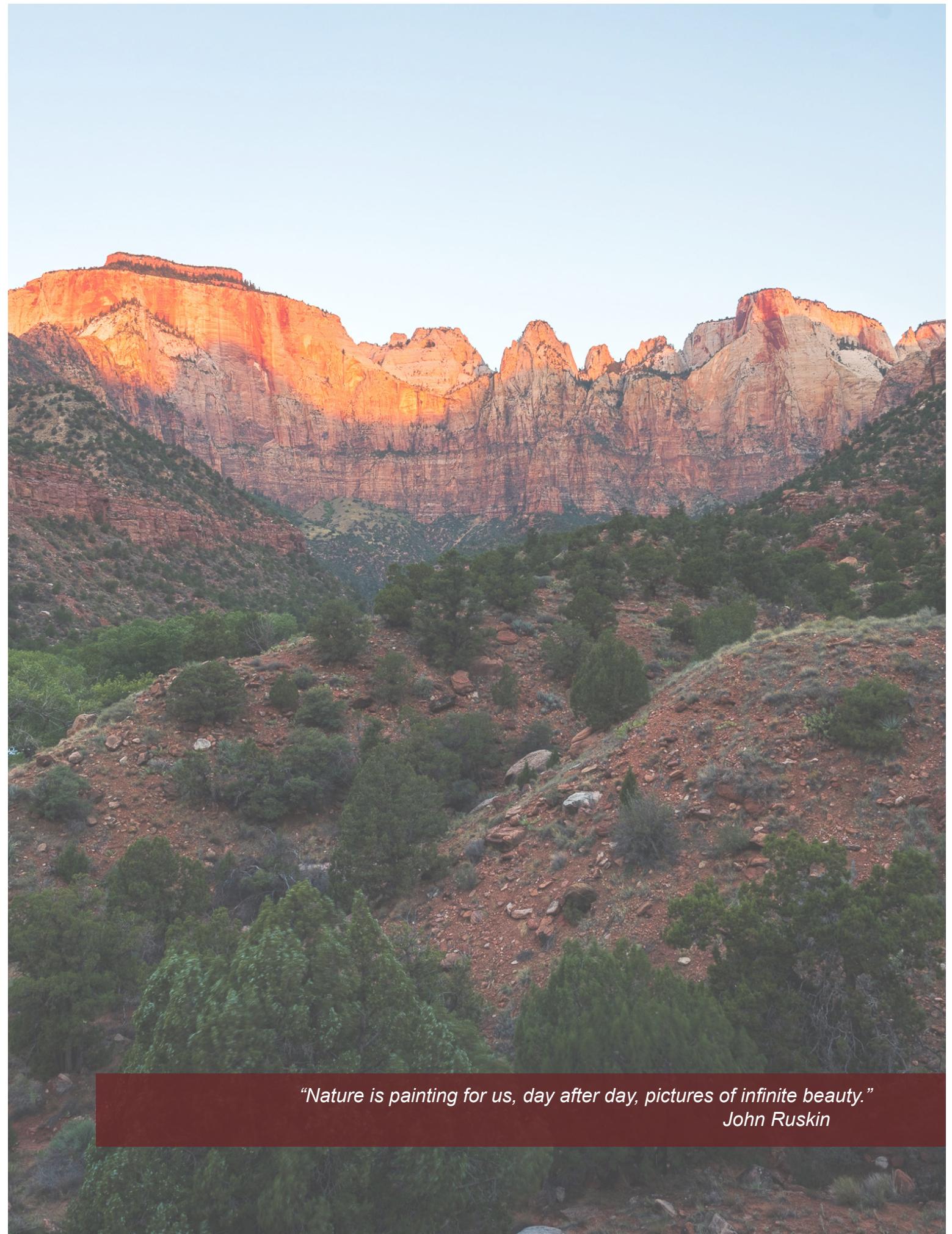
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## GRASSES CONT.

Botanical Name	Common Name	Size (H x W)	Image
<i>Sporobolus airoides</i>	Alkali Sacaton	3-4' x 2-3'	
<i>Stipa speciosa</i>	Desert Needlegrass	1-2' x 1-2'	





*“Nature is painting for us, day after day, pictures of infinite beauty.”*  
John Ruskin



## 06. SR-9 PROPERTY CONCEPTS

The Town-owned vacant property along SR-9 provides an opportunity to create a treasured community asset that will significantly enhance Springdale's overall streetscape environment. Located to the southwest of Best Western Plus Zion Canyon Inn & Suites, the property offers views of the surrounding peaks and is within the heart of downtown. While this property is intended to be enjoyed by both residents and visitors, the goal of the SR-9 property design is to create a space that can support community-specific needs and events such as Farmer's Markets, small group gatherings, and education.

The results of the public outreach indicate that the community is very interested in incorporating interpretive signage, educational opportunities, passive seating areas, and hosting small community events within the property. The community was also strongly supportive of including a restroom in the design, which reflects the desire to better accommodate Springdale's visitors. There was also interest in including a cooling water feature; however, there was equal concern over unnecessary water usage. The concepts depicted on the following pages show several options for how this property can be transformed to reflect the needs and wants of the community. While further detailed design is required to transform the preferred concept into reality, the Town is well-poised to turn this vacant plot of land into a community treasure.



*Existing SR-9 Property*

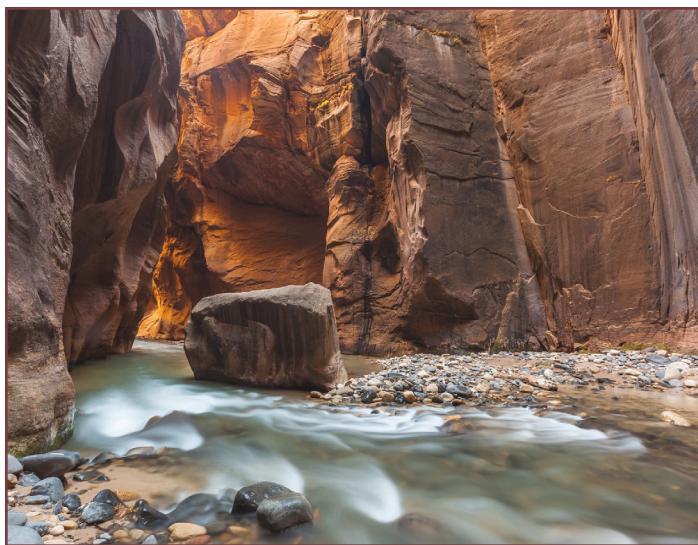
## PRELIMINARY CONCEPT - 1: SPRINGDALE GEOLOGY

The Springdale Geology Concept features accent materials inspired by the unique geology and inspiring rock formations of the area. Sandstone pavers, colored concrete, and granite accent boulders play off of the serene natural beauty surrounding Springdale.

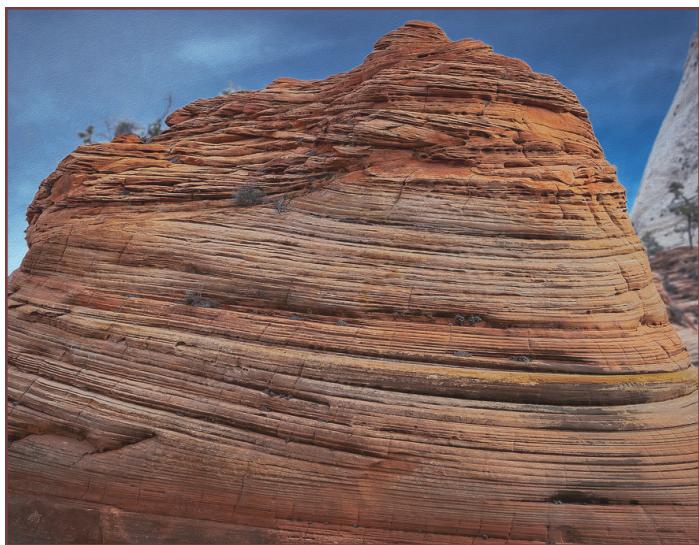
The patio space provides opportunity for shaded seating, informal gatherings, picnics, and small events, while allowing visitors to learn about the region's geology through a series of interpretive signage. A connection is provided to the adjacent lawn to allow for additional recreational opportunities and additional shaded seating.



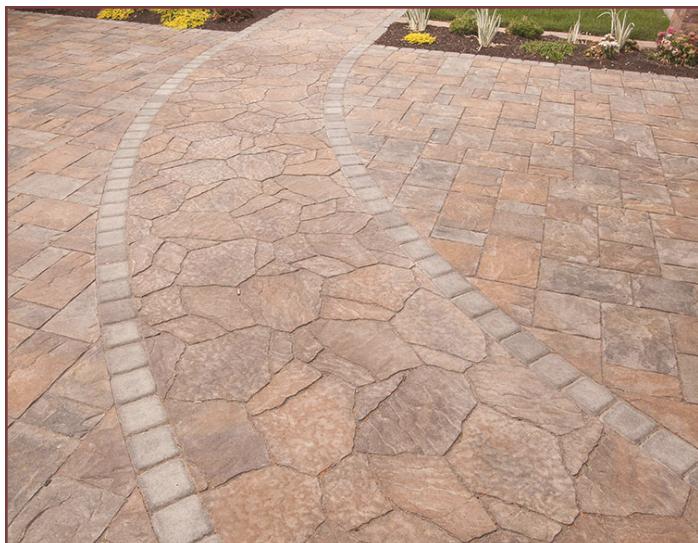
Preliminary Concept 1 Plan



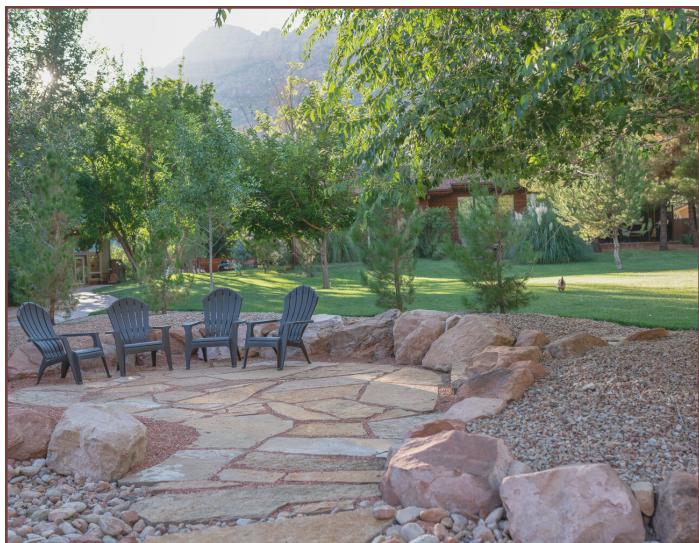
*Zion Narrows Floating Rock*



*Sandstone Rock Formations*



*Textured Accent Bands with Patio Space*



*Natural Seating Area*



*Patio Tables and Chairs with Shade Canopies*



*Interpretive Signage*

## PRELIMINARY CONCEPT - 2: SANDSTONE CANYON

The Sandstone Canyon Concept is inspired by the dramatic peaks that soar above Springdale and the sharp, fragmented angles showcased on the rock faces. The layout includes strong geometric forms paired with decorative steel to create a contemporary feel in the space.

The patio features locations to showcase local artwork, a tiered, cascading water feature, shade trees, seating areas, formal plantings and interpretive signage. Formal plantings help to reinforce the geometric forms of the space. The placement of the restroom building helps to create a buffer between SR-9 and the seating spaces.



Preliminary Concept 2 Plan



The Three Patriarchs



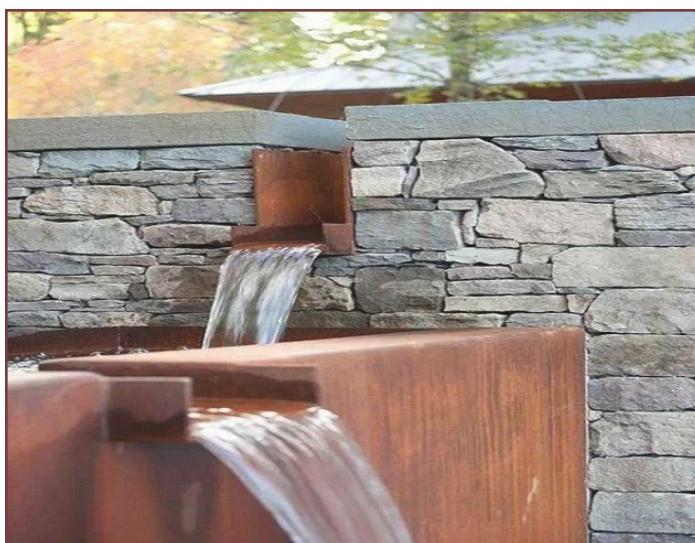
Local Artwork



Formal Plantings with Concrete Seatwalls



Decorative Steel Accents



Tiered Water Feature



Interpretive Signage

## PRELIMINARY CONCEPT - 3: THE VIRGIN RIVER

The Virgin River Concept takes visitors on a journey along the Virgin River and its numerous tributaries, highlighting the role that water has had in shaping the land and its importance to the adjacent communities.

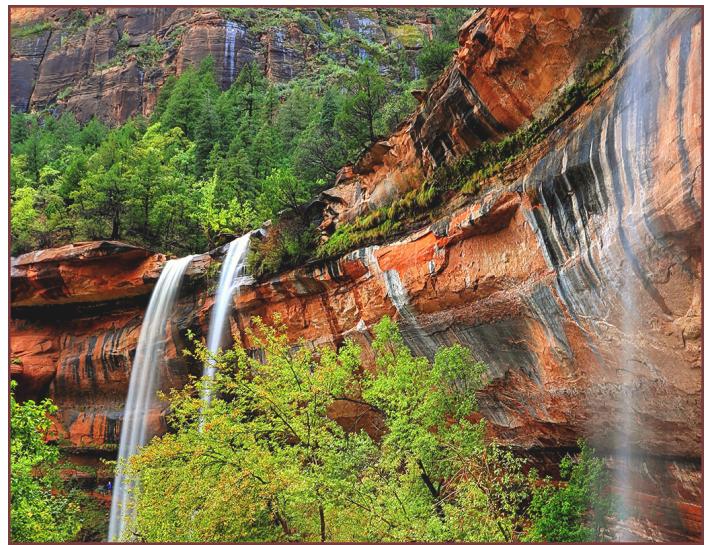
A curved shade structure reminiscent of a grotto provides comfortable seating next to a cooling bubbling spring water feature. Embedded in the hardscape are decorative medallions depicting the locations of nearby communities. The patio also features interpretive signage highlighting natural and cultural aspects of the surrounding area, a restroom, and ample space for small community events.



Preliminary Concept 3 Plan



The Virgin River



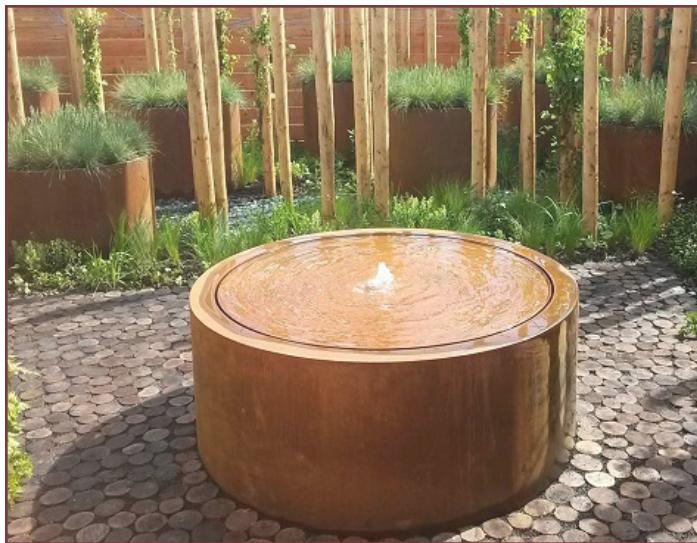
The Grotto at Emerald Pools



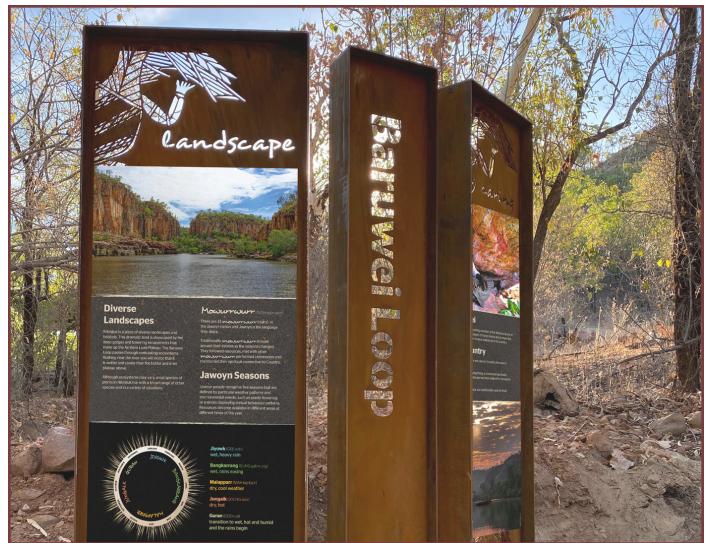
Pavement Markings Highlighting the River



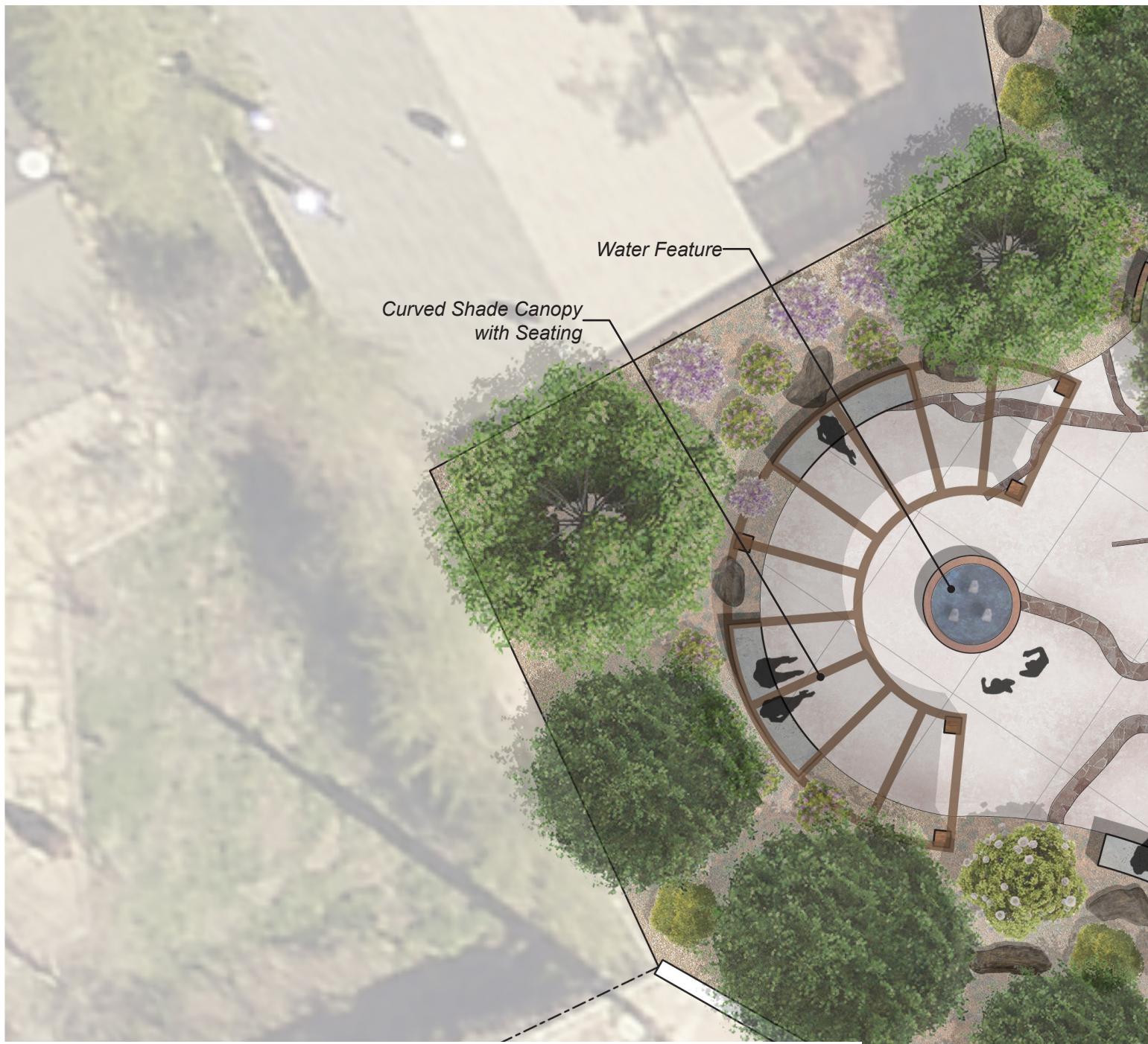
Curved Shade Canopy with Seating



Water Feature



Interpretative Signage



Preferred Concept Plan

## PREFERRED DESIGN CONCEPT: THE VIRGIN RIVER

The preferred design concept is a refined version of The Virgin River concept based on input from the Springdale Streetscape Committee. The restroom location has been adjusted to minimize potential impacts to adjacent properties while the curved canopy has been simplified to maximize cost effectiveness. The pavement has also been adjusted to better integrate with the adjacent sidewalk area. The remainder of the concept remains the same, including the small water feature, decorative medallions and pavement markings depicting the Virgin River, interpretive signage, and ample space for small community events.



